

*Brown*

# **Pony Farm Road**

# **CP Rail Bridge**

Presented to Town of Oneonta Board

February 13, 2008



Feb. 12, 2008

We would like to thank you for meeting with us on Friday Afternoon Feb. 8, 2008.

Summarizing the meeting:

We discussed the public safety issues of Pony Farm Rd.

We discussed the ingress and egress to Pony Farm Rd., particular attention to the section west of the Otego Creek Bridge.

We discussed the businesses @ 330 Pony Farm Rd. that have been seriously affected by the decision to close the CP Rail Bridge.

**Bob Wood:**

- **Town position on fixing the CP Rail Bridge and billing CP Rail-**
  - **Bob to speak to the town attorney**
  - **Town to contact the engineer**
  - **Corrective action to bring the bridge back to a temporary safe status**
- **A letter to Governor Spitzer**
  - **Asking for additional funds to upgrade Pony Farm Rd.**
- **Move the barricades closer to the intersections- coordinated effort with State DOT**
  - **Contact the county about the Pony Farm RD signs used in the detour**
- **Town to upgrade the road from a farm road**
  - **Contact NYSEG about moving Utility Poles to back of right of way**
  - **Start filling in shoulders an low areas along roadway**

**Gerald Murello:**

- **Send VP the Electronic file of CP Rail Bridge as built drawings**
  - **Supply to Bob Wood for Engineers**
- **Construct detour signs**
  - **West of CP Rail Bridge on route 7 directing traffic through Industrial Park**
  - **Intersection of Route 205 from three sides- route 205 so. Route 7 east and west**
  - **Route 205 from I88 before Corporate Dr.**

**CommInnovations:**

- **Supply copies of as built drawing to town of Oneonta**
  - **Engineers to use for study and affect a viable repair to CP Rail Bridge**
  - **Supply a signed petition to the Town of Oneonta from residents and businesses affected by closing of CP Rail Bridge**

Thank you,

A handwritten signature in black ink, appearing to read "Brian Konze".

**Brian Konze**

**February 8, 2008**

**Subj: Bridge Safety & Access to our Neighborhood**

**Hello,**

**We are all sad to see the Rt 7 / Pony Farm Road bridge connecting our community along the western end of Pony farm Road was recently closed. This closure represents more than just an inconvenience.**

**The closure of this bridge creates a safety concern & will have a financial impact through lower property values & possibly higher insurance premiums. The bridge on Pony Farm Road across Otego Creek is our principal access & there is one at-grade railroad crossing that can be used in emergency. The largest concern is access for fire & other emergency vehicles.**

**We learned in the summer of 2006, the Otego Creek bridge was built with far too little clearance over the creek & is susceptible to flood damage & can be closed for long periods waiting for repair. Furthermore, the section of road approaching this bridge from the east is very low and easily flooded in storms of less severity than 2006. Also, in heavy snow storms this section of road is impassible for long periods of time. The at-grade emergency crossing is often blocked for long periods of time by stopped trains.**

**In September 2006 we were pleased to hear that an agreement had been reached that would result in this bridge being replaced in the summer of 2008. Also in response to a resident's petition in November 2006, the Town of Oneonta agreed to install one or two lights when the new bridge is installed. Since that time the date has been pushed back to the summer of 2009 for the new bridge & lighting. Worse yet, based on the current lack of progress this date appears to be in great jeopardy.**

**The Rt 7 / Pony Farm Road bridge effectively represents the entrance to the Town of Oneonta for those traveling west on Rt 7. It has long been an eyesore rather than a gateway to our town we could be proud of. Please express your concern & support for action by signing the attached petition.**

**You can call Mike Adams or Jerry Murillo at the DOT (432-4810) if you wish to endorse the improved signage and lighting along Route 7.**

**A Town Board meeting is held 7:30 pm at the Town Hall (West Oneonta) the second Wednesday of every month. If you are unavailable to attend: please call, write or e-mail the Town Board Members.**

**Your concerned neighbor, Bill Castine**

February 8, 2008 (continuation)

Petition: Pony Farm Road Residents Safety ~ Emergency Vehicle Access

We the undersigned petition the Town Board of Oneonta to recognize our concern with regards to the closure of the Rt 7 / Pony Farm Road bridge & to take action to resolve this problem.

The closure of this bridge creates a safety concern & will have a financial impact through lower property values & possibly higher insurance premiums. The bridge on Pony Farm Road across Otego Creek is our principal access & there is one at-grade railroad crossing that can be used in emergency. The largest concern is access for fire & other emergency vehicles.

The Otego Creek bridge was built with too little clearance over the creek & is susceptible to flood damage & can be closed for long periods waiting for repair. The section of road approaching this bridge from the east is very low and easily flooded in storms of less severity than 2006 & during heavy snow storms this section of road is impassible for long periods of time. The at-grade emergency crossing is often blocked for long periods of time by stopped trains.

Name (Print)	Address	Signature	Date
Comm INNOVATIONS	330 Pony Farm Rd	[Signature]	2/11/08
DTI	330 Pony Farm Rd	[Signature]	2/11/2008
Chase, Charlotte	330 Pony Farm Rd	[Signature]	2/11/2008
Chase, Chris	330 Pony Farm Rd	[Signature]	2/11/08
Tim Page	173 Mill Creek Rd	[Signature]	2/11/08
Jenny Hahn	330 Pony Farm Rd	[Signature]	2/13/08
GREG SPARCO	330 Pony Farm Rd	[Signature]	2/13/08
MICHAEL RYDER	330 Pony Farm Rd	[Signature]	2/13/08
Jason Thore	" " " "	[Signature]	2/13/08
Elizabeth Coffin	330 Pony Farm Rd	[Signature]	02/13/08
Colin Thorstensen	330 Pony Farm Rd	[Signature]	02/13/08
Mathia Coughlan	330 Pony Farm Rd	[Signature]	2/13/08
David Morrill	330 Pony Farm Rd	[Signature]	2/13/08
John Stocka	330 Pony Farm Rd.	[Signature]	2/13/08

February 8, 2008

**Petition: Pony Farm Road Residents Safety ~ Emergency Vehicle Access**

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Name of Resident	Address	Signature	Date
<u>Rich Babbitt</u>	<u>329 Pony Farm Road</u>	<u>[Signature]</u>	<u>2-8-08</u>
<u>HOWARD L. TRASK</u>	<u>362 PONY FARM ROAD</u>	<u>Howard L. Trask</u>	<u>2/9/08</u>
<u>Deborah A. Trask</u>	<u>362 Pony Farm Road</u>	<u>Deborah A. Trask</u>	<u>2/9/08</u>
<u>Mark Lager</u>	<u>366 Pony Farm Rd</u>	<u>M. Lager</u>	<u>2/9/08</u>
<u>Jacy Lager</u>	<u>366 Pony Farm Rd</u>	<u>S. Lager</u>	<u>2/9/08</u>
<u>Jessie Woodcock</u>	<u>153 main St. Delhi</u>	<u>J. Woodcock</u>	<u>2/9/08</u>
<u>Raymond Hamilton</u>	<u>398 Pony Farm Road</u>		
<u>Betty Waters</u>	<u>372 Pony Farm Rd</u>	<u>Betty Waters</u>	<u>2/9/08</u>
<u>Charles Dyer</u>	<u>398 Pony Farm Rd</u>	<u>Charles Dyer</u>	<u>2/9/08</u>
<u>David F. Matlew</u>	<u>298 Pony Farm Rd</u>	<u>David F. Matlew</u>	<u>2/9/08</u>
<u>John Jones</u>	<u>116 West Delly St Oneonta</u>	<u>John Jones</u>	<u>2/9/08</u>
<u>JAMES ASTINE</u>	<u>330 Pony Farm Rd</u>	<u>J. Astine</u>	<u>2/10/08</u>
<u>Paul M. Mattick</u>	<u>333 Pony Farm Rd</u>	<u>Paul M. Mattick</u>	<u>2/11/08</u>
<u>Bob Mattick</u>	<u>311 Pony Farm Rd</u>	<u>Bob Mattick</u>	
<u>Jan Knapp</u>	<u>330 Pony Farm Rd</u>	<u>Jan Knapp</u>	<u>2-11-08</u>
<u>Jason Knapp</u>	<u>330 Pony Farm Rd</u>	<u>Jason Knapp</u>	<u>2-11-08</u>
<u>AutoTech Mobile Electronics</u>	<u>330 Pony Farm Rd</u>	<u>[Signature]</u>	<u>2-11-08</u>
<u>Melina Feltz</u>	<u>15 Schoster St Oneonta</u>	<u>Melina Feltz</u>	<u>2-11-08</u>

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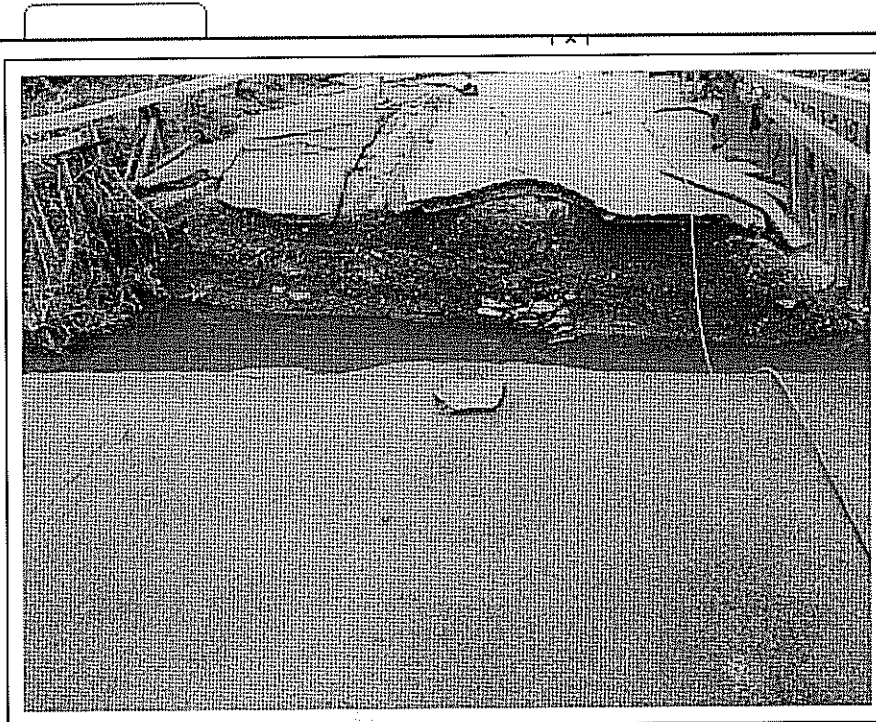
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Photos from PonyFarm Road and the bridge on Route 7.



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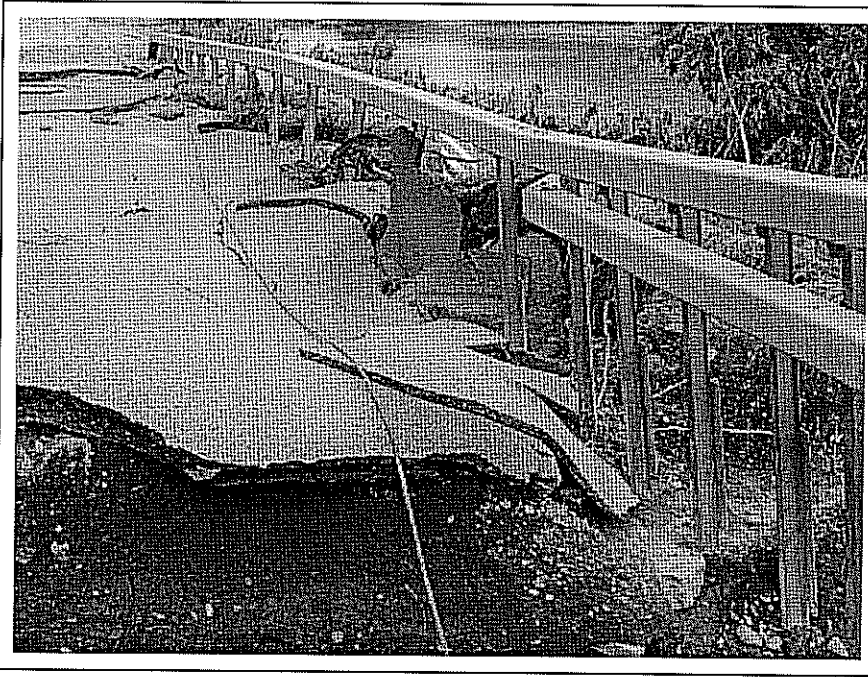
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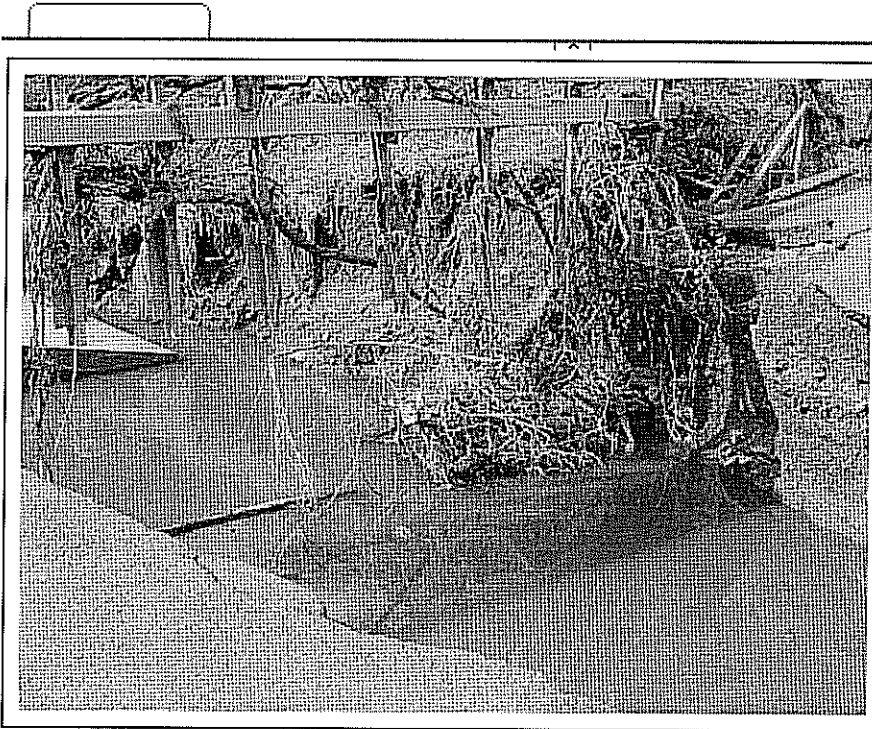
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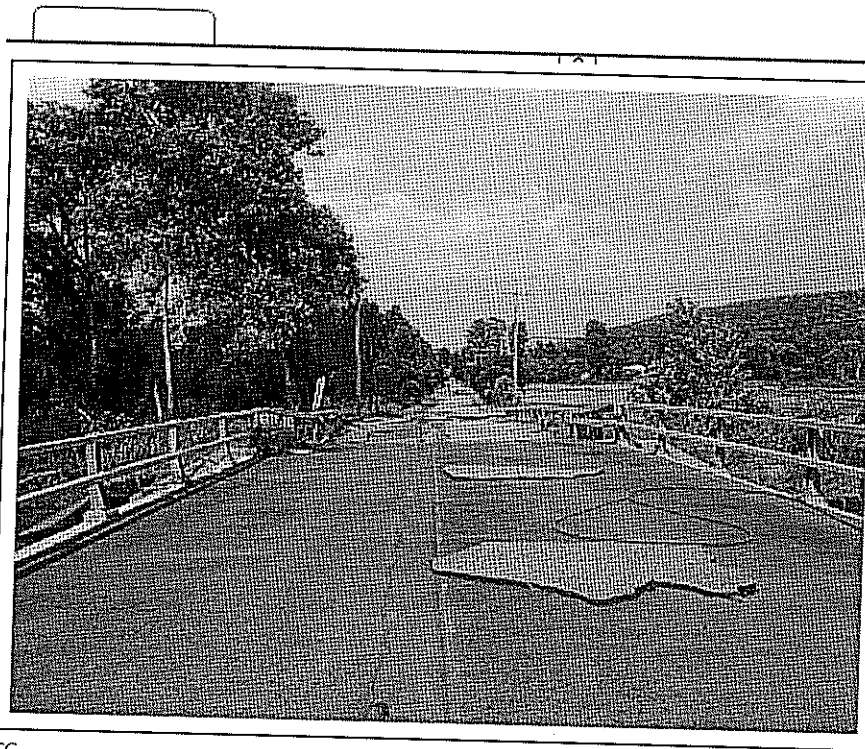
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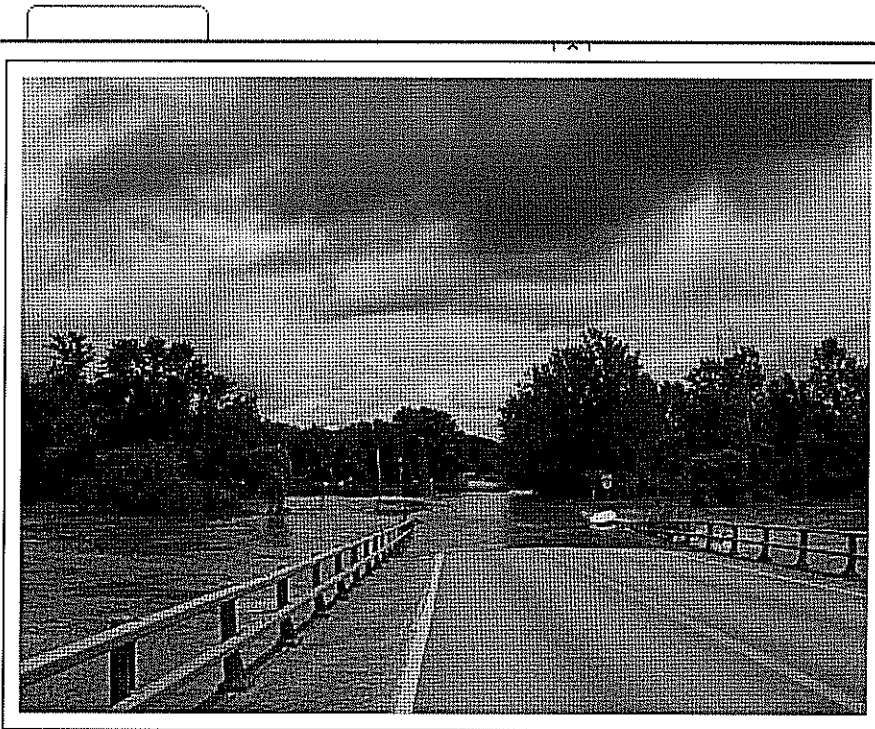
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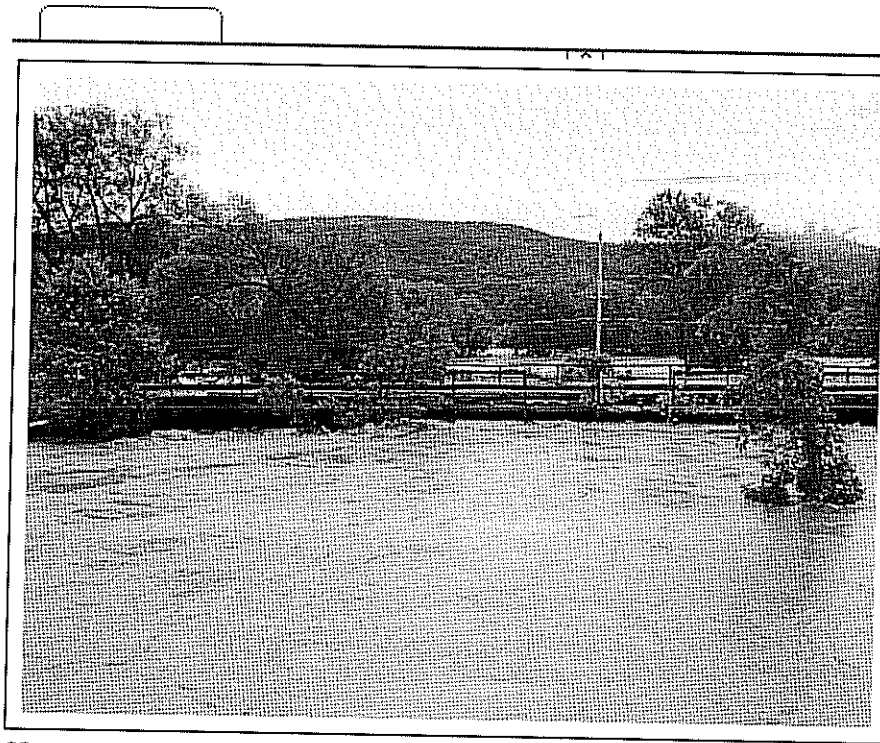
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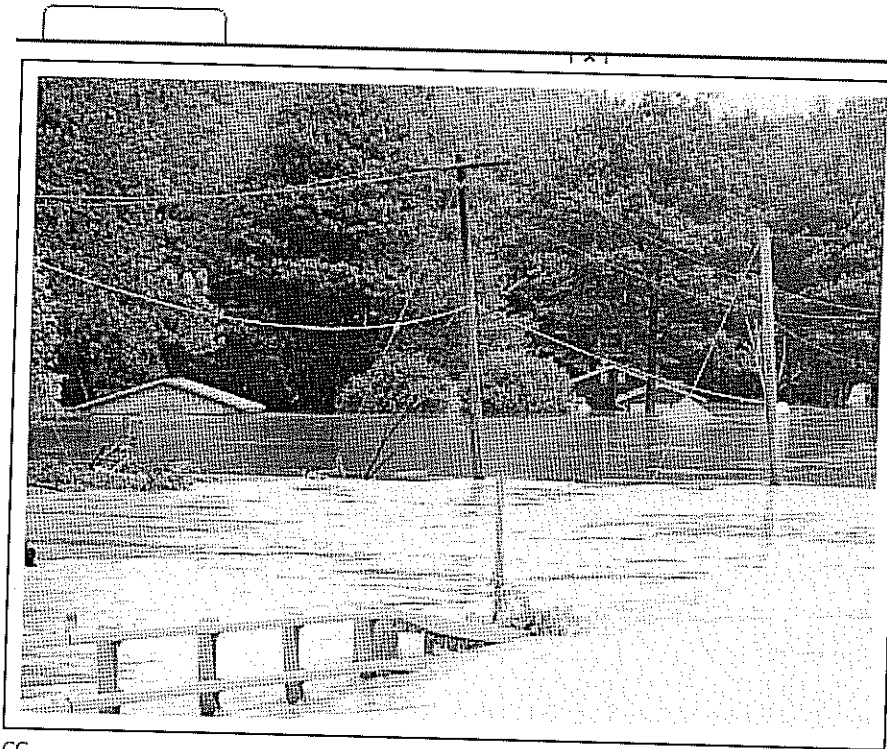
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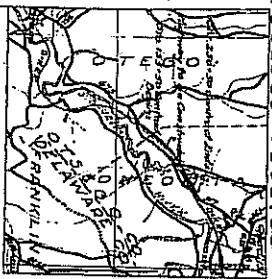
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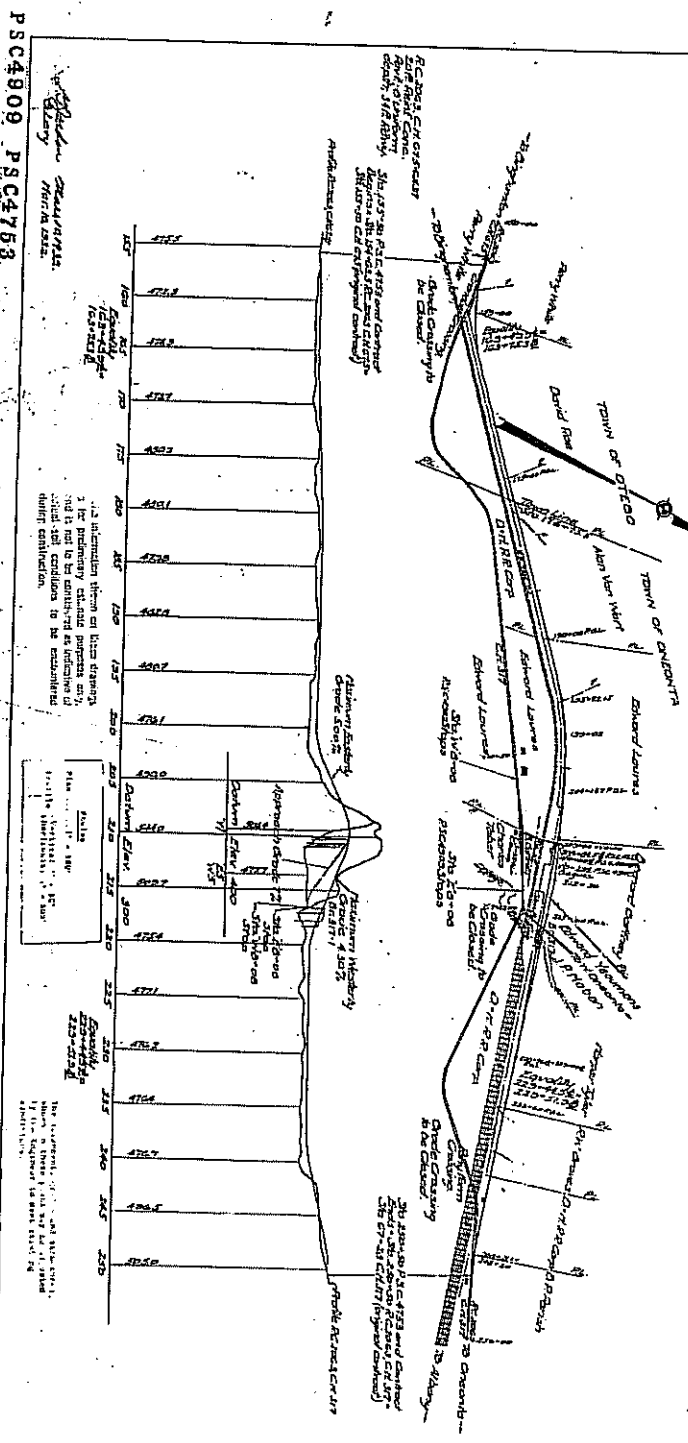


STATE OF NEW YORK  
DEPARTMENT OF PUBLIC WORKS  
DIVISION OF ENGINEERING  
PLAN FOR GRADE CROSSING EXAMINATION



OTSEGO-ONEONTA COUNTY HIGHWAY NO. 675  
SHADY SIDE-OTSEGO COUNTY HIGHWAY NO. 317  
WEST ONEONTA ROAD  
AND

THE DELAWARE AND HUDSON RAILROAD  
IS HEREIN  
TOTAL LENGTH OF CONTRACT, 2.55 MILES  
OTSEGO COUNTY  
N. S. C. 1113 and 1100



Scale: 1" = 100'  
Vertical Scale: 1" = 10'

The information shown on these drawings is based on the field notes and other data furnished to the Engineer by the contractor and is not to be construed as a guarantee of the accuracy of the same or as a warranty of the contractor's performance.

Scale: 1" = 100'  
Vertical Scale: 1" = 10'

The information shown on these drawings is based on the field notes and other data furnished to the Engineer by the contractor and is not to be construed as a guarantee of the accuracy of the same or as a warranty of the contractor's performance.

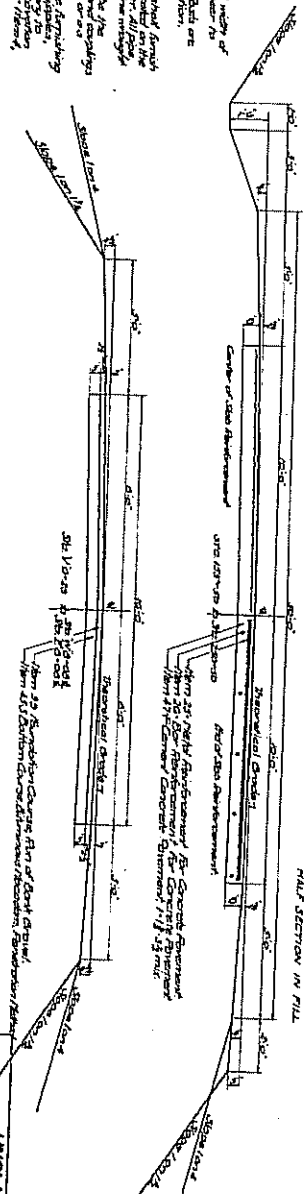
Sheet No.	Description	Scale
1	GENERAL NOTES	1" = 100'
2	PLAN	1" = 100'
3	ELEVATION	1" = 10'
4	CROSS SECTION	1" = 10'
5	PROFILES	1" = 10'

TYPE OF CONSTRUCTION  
REINFORCED CONCRETE PAVEMENT, 11'-0" x 17'-0" x 12" MAX.  
BIRMINGHAM CONCRETE PAVEMENT, 11'-0" x 17'-0" x 12" MAX.  
SIX INCH STEEL DECKING, 11'-0" x 17'-0" x 12" MAX.  
SIX INCH STEEL DECKING, 11'-0" x 17'-0" x 12" MAX.  
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SIX INCH STEEL DECKING, 11'-0" x 17'-0" x 12" MAX.

Microfilm Notes:  
Sheets 2, 3, 4, 5 were standard structure  
Sheets 1, 6, 7, 8 are not included in this set.

Approved: *David Hooper*  
Superintendent of Public Works  
Approved by order of the Public Works Board:  
*John E. Jones*  
Approved by the Delaware and Hudson R.R. Co.  
*Walter B. Smith*  
Approved by the New York Department of Public Works:  
*John E. Jones*  
Approved by the Otsego County Board of Supervisors:  
*John E. Jones*  
Approved by the Oneonta City Board of Supervisors:  
*John E. Jones*

TYPICAL SECTIONS  
Sheet 578



Sheet	Area	Volume
104103	1.2	1200
104104	1.2	1200
104105	1.2	1200
104106	1.2	1200
104107	1.2	1200
104108	1.2	1200
104109	1.2	1200
104110	1.2	1200
104111	1.2	1200
104112	1.2	1200
104113	1.2	1200
104114	1.2	1200
104115	1.2	1200
104116	1.2	1200
104117	1.2	1200
104118	1.2	1200
104119	1.2	1200
104120	1.2	1200

P 5 C 4 9 0 9 P 5 C 4 7 8 3

Notes: The notes and details of this and other sheets of this set shall be read in conjunction with the specifications for the work to be performed. The notes and details of this and other sheets of this set shall be read in conjunction with the specifications for the work to be performed. The notes and details of this and other sheets of this set shall be read in conjunction with the specifications for the work to be performed.

Structure	Structure	Size	Remarks
104103	Abutment	10' x 10'	Abutment
104104	Abutment	10' x 10'	Abutment
104105	Abutment	10' x 10'	Abutment
104106	Abutment	10' x 10'	Abutment
104107	Abutment	10' x 10'	Abutment
104108	Abutment	10' x 10'	Abutment
104109	Abutment	10' x 10'	Abutment
104110	Abutment	10' x 10'	Abutment
104111	Abutment	10' x 10'	Abutment
104112	Abutment	10' x 10'	Abutment
104113	Abutment	10' x 10'	Abutment
104114	Abutment	10' x 10'	Abutment
104115	Abutment	10' x 10'	Abutment
104116	Abutment	10' x 10'	Abutment
104117	Abutment	10' x 10'	Abutment
104118	Abutment	10' x 10'	Abutment
104119	Abutment	10' x 10'	Abutment
104120	Abutment	10' x 10'	Abutment

Station	Structure	Remarks
104103	Abutment	Abutment
104104	Abutment	Abutment
104105	Abutment	Abutment
104106	Abutment	Abutment
104107	Abutment	Abutment
104108	Abutment	Abutment
104109	Abutment	Abutment
104110	Abutment	Abutment
104111	Abutment	Abutment
104112	Abutment	Abutment
104113	Abutment	Abutment
104114	Abutment	Abutment
104115	Abutment	Abutment
104116	Abutment	Abutment
104117	Abutment	Abutment
104118	Abutment	Abutment
104119	Abutment	Abutment
104120	Abutment	Abutment

Station	Structure	Remarks
104103	Abutment	Abutment
104104	Abutment	Abutment
104105	Abutment	Abutment
104106	Abutment	Abutment
104107	Abutment	Abutment
104108	Abutment	Abutment
104109	Abutment	Abutment
104110	Abutment	Abutment
104111	Abutment	Abutment
104112	Abutment	Abutment
104113	Abutment	Abutment
104114	Abutment	Abutment
104115	Abutment	Abutment
104116	Abutment	Abutment
104117	Abutment	Abutment
104118	Abutment	Abutment
104119	Abutment	Abutment
104120	Abutment	Abutment

Station	Structure	Remarks
104103	Abutment	Abutment
104104	Abutment	Abutment
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104107	Abutment	Abutment
104108	Abutment	Abutment
104109	Abutment	Abutment
104110	Abutment	Abutment
104111	Abutment	Abutment
104112	Abutment	Abutment
104113	Abutment	Abutment
104114	Abutment	Abutment
104115	Abutment	Abutment
104116	Abutment	Abutment
104117	Abutment	Abutment
104118	Abutment	Abutment
104119	Abutment	Abutment
104120	Abutment	Abutment

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104105	Abutment	Abutment
104106	Abutment	Abutment
104107	Abutment	Abutment
104108	Abutment	Abutment
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104111	Abutment	Abutment
104112	Abutment	Abutment
104113	Abutment	Abutment
104114	Abutment	Abutment
104115	Abutment	Abutment
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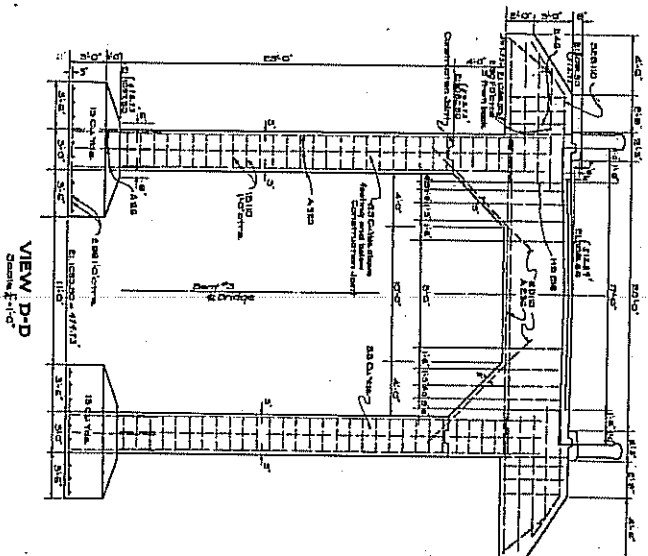
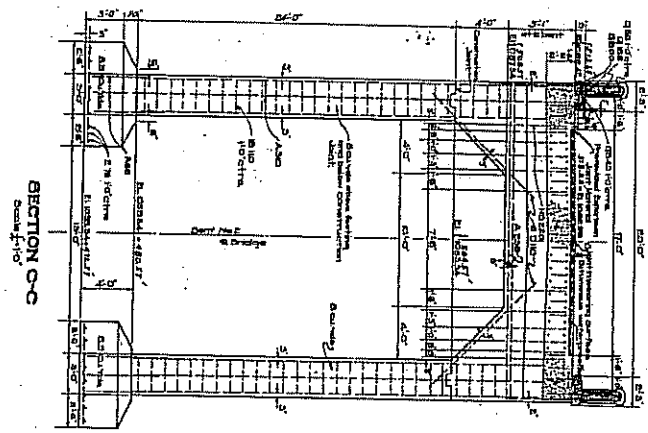
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PSC4909



**BAR LIST**

NO.	DESCRIPTION	QUANTITY	UNIT	REMARKS
1	Vertical bars	110	ft	110' x 12" dia
2	Horizontal bars	110	ft	110' x 12" dia
3	Diagonal bars	110	ft	110' x 12" dia
4	Reinforcing ties	110	ft	110' x 12" dia
5	Concrete	286	cubic	286 cubic
6	Formwork	78	sq ft	78 sq ft
7	Reinforcing steel	495	lbs	495 lbs
8	Steel fabric reinforcement	200	sq ft	200 sq ft

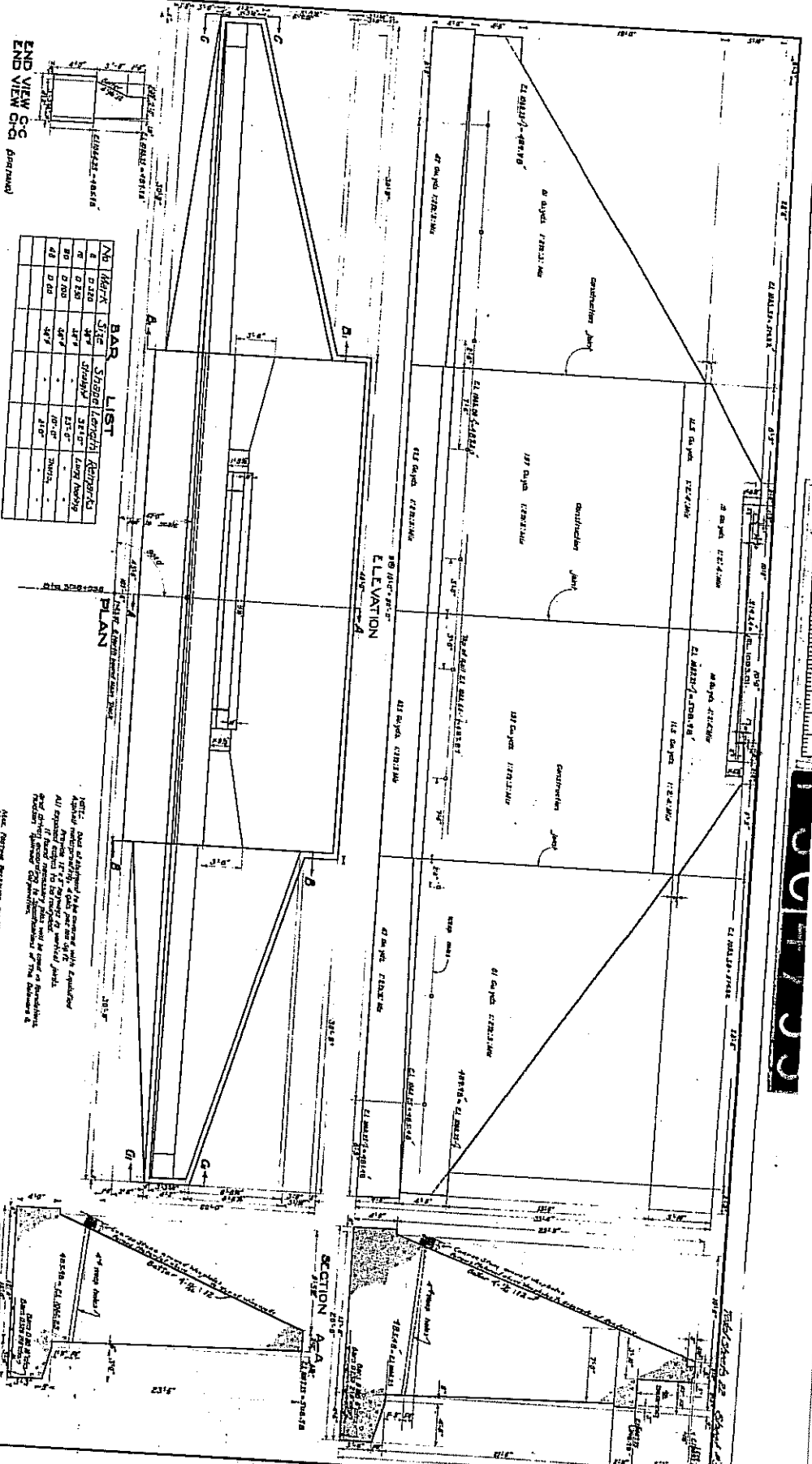
**Notes:**  
 Higher dimensions apply to R.R. Deck  
 Lower dimensions apply to 27'11" from deck  
 unless specified - 27'11" from deck

**ESTIMATED QUANTITIES**  
 Concrete in structure 286 cubic  
 Reinforcing steel 495 lbs  
 Steel fabric reinforcement 200 sq ft  
 Foundation: Elevation 200 sq ft

**EAST APPROACH  
 WEST ONEONTA ROAD  
 ONEONTA NY**  
 THE DELAWARE & HUDSON RAILROAD  
 PROPOSED EXAMINATION OF PONY PIER  
 DRANDALL AND WEST ONEONTA ROAD BRIDGE  
 425 N. 100 ST. SOUTH OF  
 ONEONTA NY

Office of Chief Engineer  
 477  
 35538





**BAR LIST**

No	Bar	Size	Shape	Quantity	Remarks
1	4#	1/2"	Round	12	Long Runners
2	4#	1/2"	Round	12	Long Runners
3	4#	1/2"	Round	12	Long Runners
4	4#	1/2"	Round	12	Long Runners
5	4#	1/2"	Round	12	Long Runners
6	4#	1/2"	Round	12	Long Runners
7	4#	1/2"	Round	12	Long Runners
8	4#	1/2"	Round	12	Long Runners
9	4#	1/2"	Round	12	Long Runners
10	4#	1/2"	Round	12	Long Runners
11	4#	1/2"	Round	12	Long Runners
12	4#	1/2"	Round	12	Long Runners

**ESTIMATED QUANTITIES**

Concrete 1720 cu yds.  
 Reinforcing Steel 125 cu yds.  
 Cement 27200  
 Gravel 2315 cu yds.  
 Unbasalifed Excavation 1045 cu yds.  
 Foundation Excavation 1600 cu yds.  
 Foundation Course R.O.B Gravel L.M. 70 cu yds.

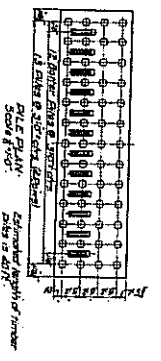
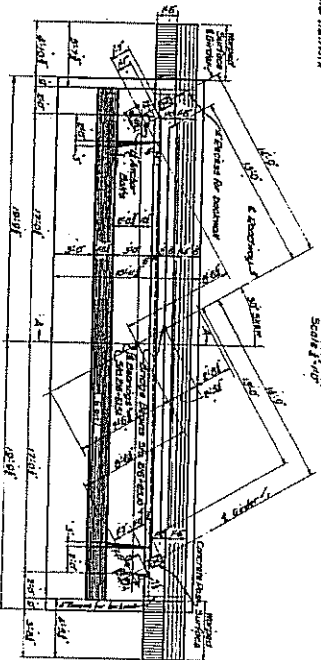
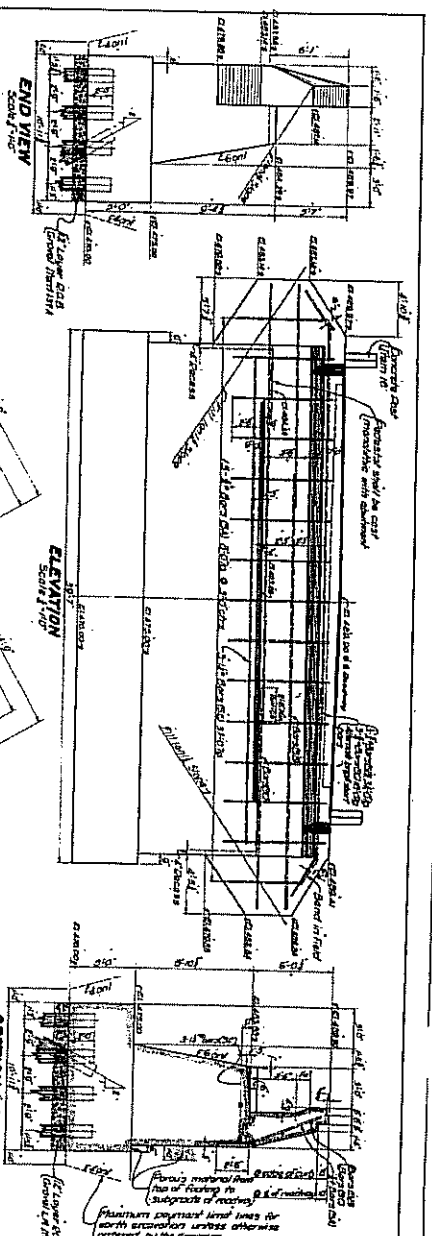
**NOTE:** Steel to be placed in concrete with expansion joints at 10' intervals. All expansion joints to be provided with expansion joints. Expansion joints to be provided at 10' intervals. Expansion joints to be provided at 10' intervals. Expansion joints to be provided at 10' intervals.

**SECTION A-A**  
 SECTION B-B  
 SECTION C-C  
 SECTION D-D  
 SECTION E-E  
 SECTION F-F  
 SECTION G-G  
 SECTION H-H  
 SECTION I-I  
 SECTION J-J  
 SECTION K-K  
 SECTION L-L  
 SECTION M-M  
 SECTION N-N  
 SECTION O-O  
 SECTION P-P  
 SECTION Q-Q  
 SECTION R-R  
 SECTION S-S  
 SECTION T-T  
 SECTION U-U  
 SECTION V-V  
 SECTION W-W  
 SECTION X-X  
 SECTION Y-Y  
 SECTION Z-Z

**ONEONTA, NY**

477 35538

Sheet	1 of 2	Total Sheets	2
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Drawn	J. E. ...	Checked	J. E. ...
Scale	1" = 10'	Scale	1" = 10'

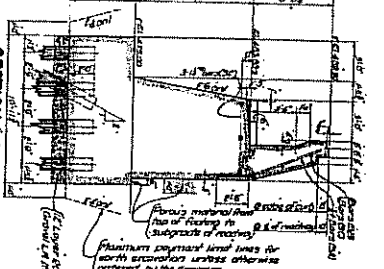


**BAR LIST**

NO.	SIZE	NO. OF BARS	DESCRIPTION
1	1/2"	12	Top reinforcement
2	1/2"	12	Bottom reinforcement
3	1/2"	12	Vertical reinforcement
4	1/2"	12	Diagonal reinforcement
5	1/2"	12	Other reinforcement

**NOTES**  
 1. All concrete for the steel abutment and the deck shall be of the same strength and quality as specified in the contract documents.  
 2. The design of the abutment shall be subject to the approval of the Engineer.

**SECTION A-A**



**TOTAL ESTIMATE OF QUANTITIES**

NO.	DESCRIPTION	UNIT	QUANTITY
1	Concrete	cu. yd.	1000
2	Reinforcing Steel	lbs.	50000
3	Formwork	sq. ft.	10000
4	Excavation	cu. yd.	500
5	Backfill	cu. yd.	1000
6	Other		

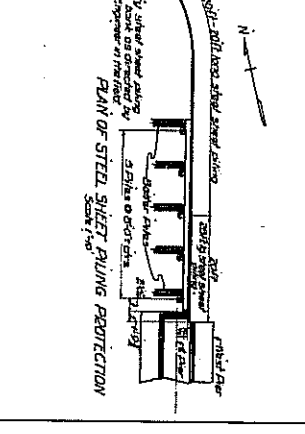
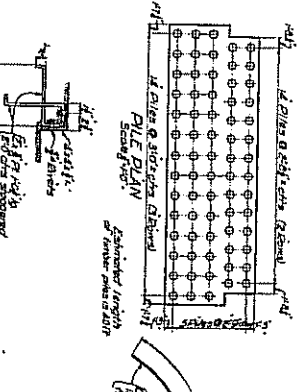
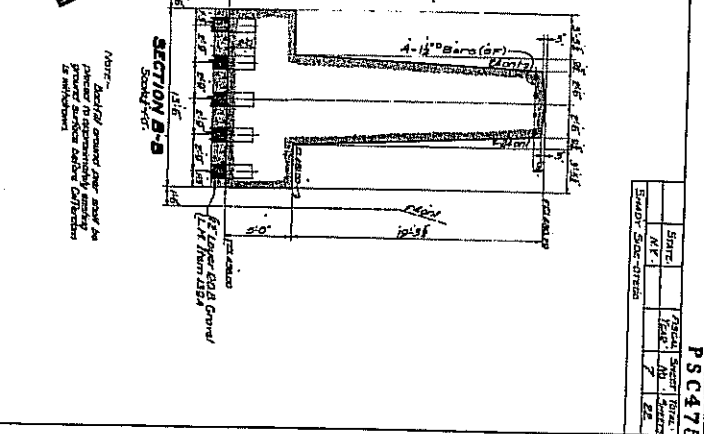
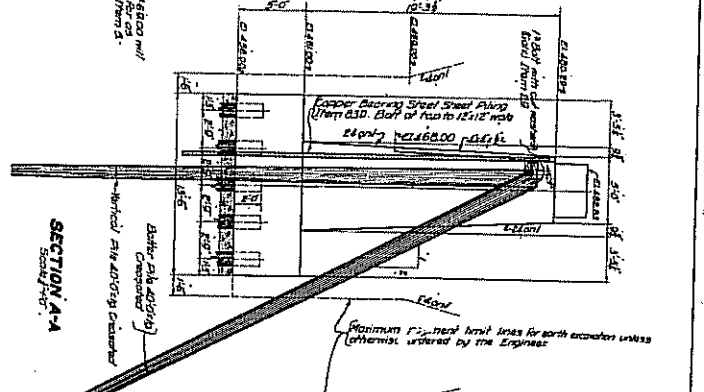
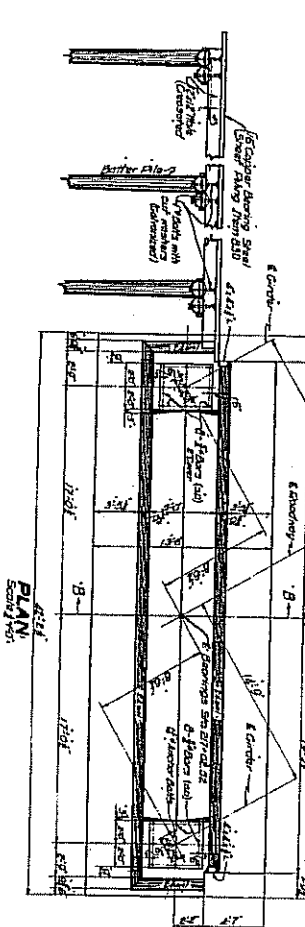
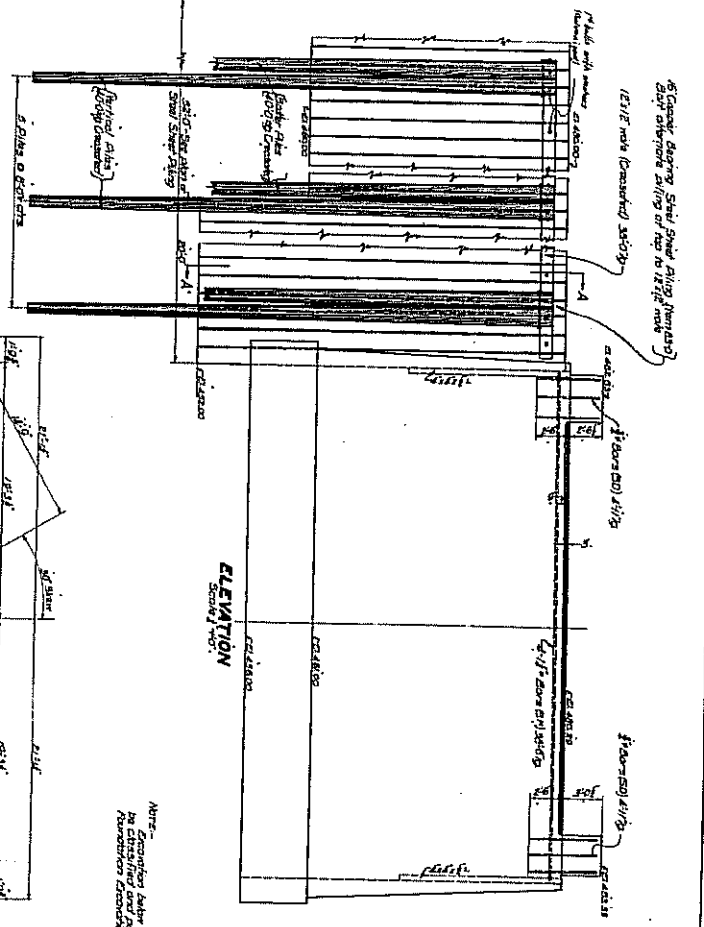
STATE OF NEW YORK  
 SHADY SIDE - OTEGO  
 BRIDGE NO. 1  
 WEST ABUTMENT

APPROVED  
 [Signature]

Drawn by: J. E. ...  
 Checked by: J. E. ...  
 In charge of the Office:  
 [Signature]

Drawn	Scale	Date	Sheet
NY			1

PSC4763



**PIER PROTECTION**  
Scale 1/4" = 1'-0"

**APPROVED**

STATE OF NEW YORK  
SHADY SIDE - OTSEGO  
BRIDGE No. 1 WEST PIER

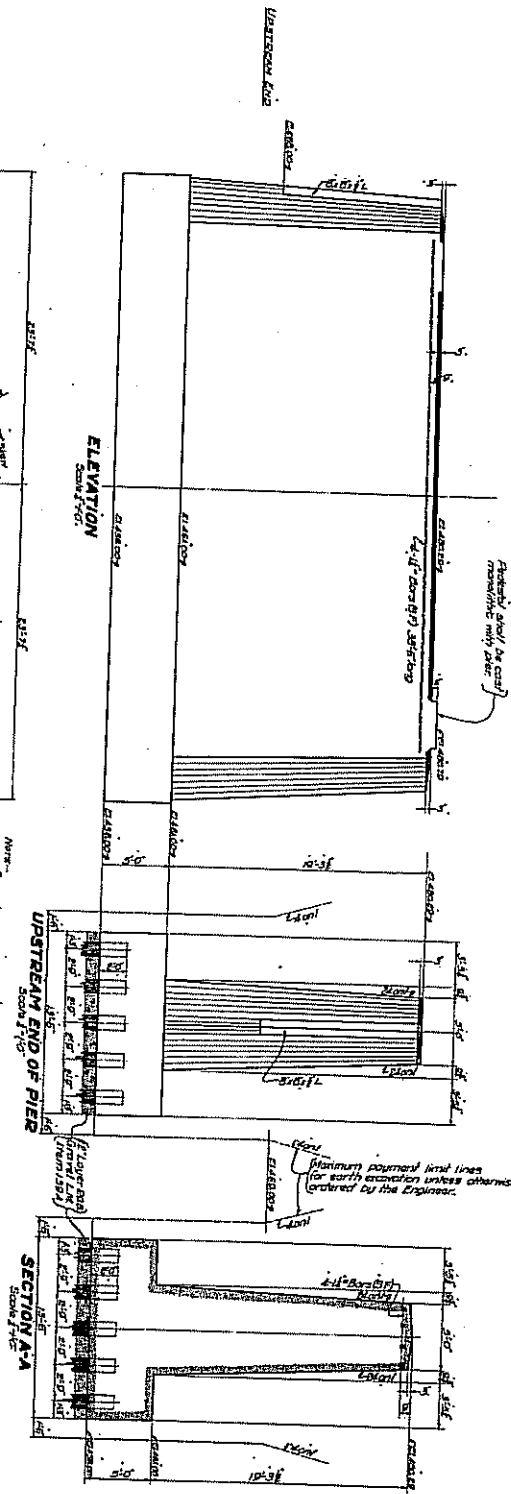
OTSEGO COUNTY  
C.H. 317  
STA. 217+4.5

Drawn by: S. J. H. G. P.  
Checked by: S. J. H. G. P.  
Project No.: 100700  
Scale: 1/4" = 1'-0"

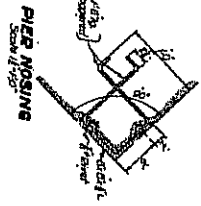
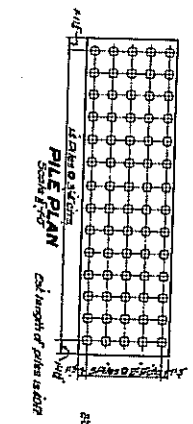
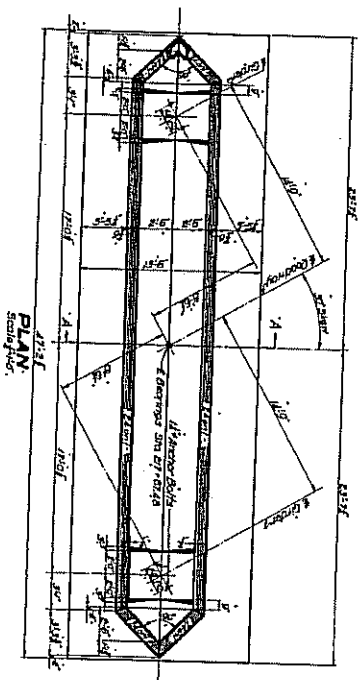
PSC4753  
 SHADY SIDE - OTSEGO  
 BRIDGE No. 1  
 EAST PIER

Sheet	PIER	PIER	PIER
PIER	PIER	PIER	PIER
SHADY SIDE - OTSEGO	SHADY SIDE - OTSEGO	SHADY SIDE - OTSEGO	SHADY SIDE - OTSEGO

PSC4753



ALL dimensions shown on this plan are  
 given in feet and inches unless  
 otherwise specified.

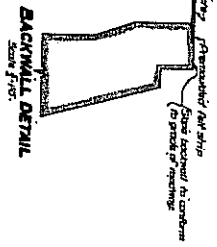
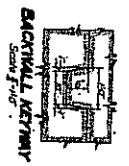
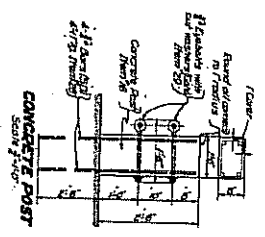
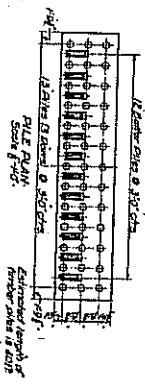
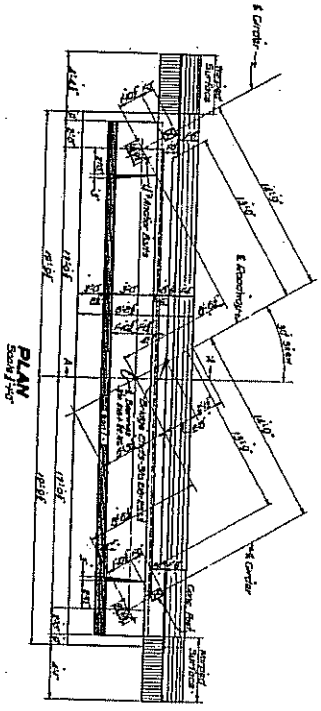
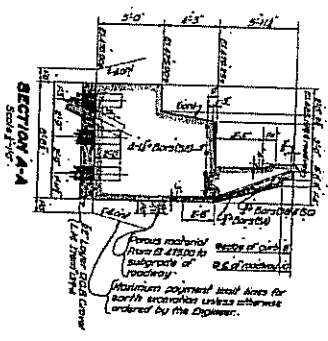
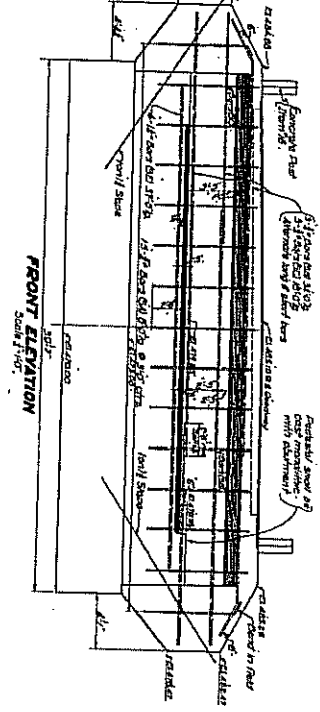
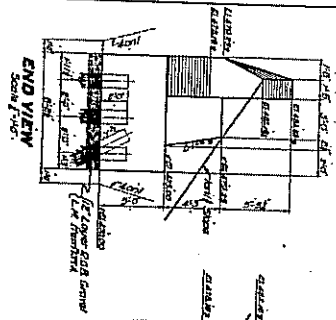


Note: Dimensions shown on Elevation will be  
 given in feet and inches unless  
 otherwise specified.

State of New York  
 Office of the State Engineer  
 Albany, N. Y.

APPROVED  
 STATE OF NEW YORK  
 SHADY SIDE - OTSEGO  
 BRIDGE No. 1  
 EAST PIER  
 OTSEGO COUNTY  
 CH. 317  
 STA. 217+45

**QUALITY**



NOTES  
 All dimensions are in feet and inches unless otherwise indicated.  
 All materials shall be of the best quality obtainable and shall conform to the specifications of the American Institute of Steel Construction, Inc. and the American Concrete Institute, Inc. unless otherwise indicated.  
 The contractor shall be responsible for obtaining all necessary permits and for the safety of the public during the construction of the bridge.

Sheet	1	Project No.	PSC4763
Scale	As Shown	Date	7/22
Drawn	Shaw	Checked	Shaw

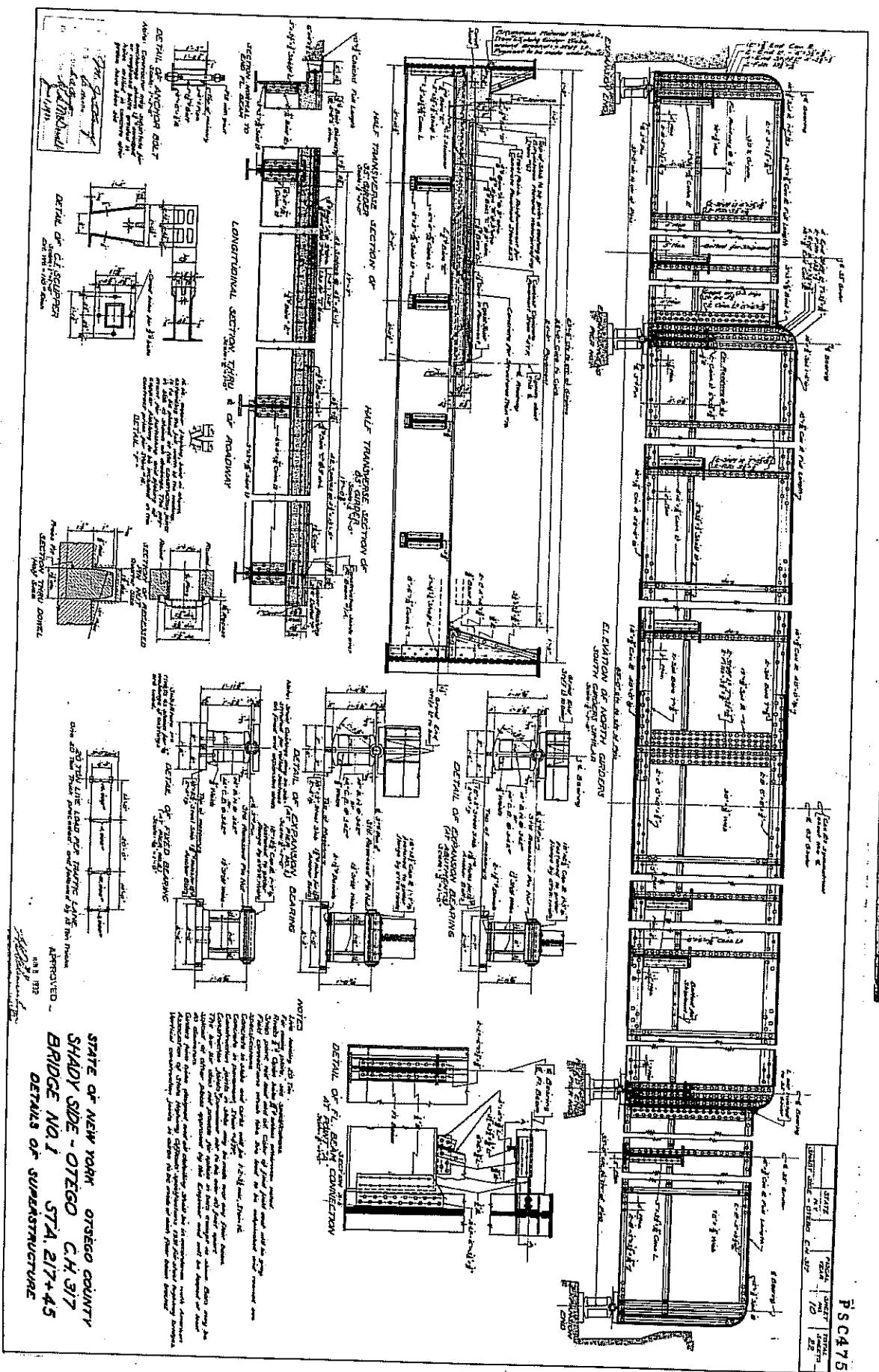
STATE OF NEW YORK  
 SHADY SIDE - OTZEO  
 BRIDGE NO. 1  
 EAST ABUTMENT

OTSEGO COUNTY  
 C.H. 317  
 STA. 217+45

APPROVED  
 J.H.S. CIV  
 [Signature]

**PLAN**

Drawn by: S.H. 1/11  
 Checked by: S.H. 1/11  
 Project No. PSC4763  
 Date: 7/22/11



**NOTES**

1. See drawing 401 for general notes.

2. The steel shall be of the best quality obtainable and shall conform to the specifications of the American Institute of Steel Constructors, Inc., and shall be of the best quality obtainable and shall conform to the specifications of the American Institute of Steel Constructors, Inc.

3. The steel shall be of the best quality obtainable and shall conform to the specifications of the American Institute of Steel Constructors, Inc.

4. The steel shall be of the best quality obtainable and shall conform to the specifications of the American Institute of Steel Constructors, Inc.

5. The steel shall be of the best quality obtainable and shall conform to the specifications of the American Institute of Steel Constructors, Inc.

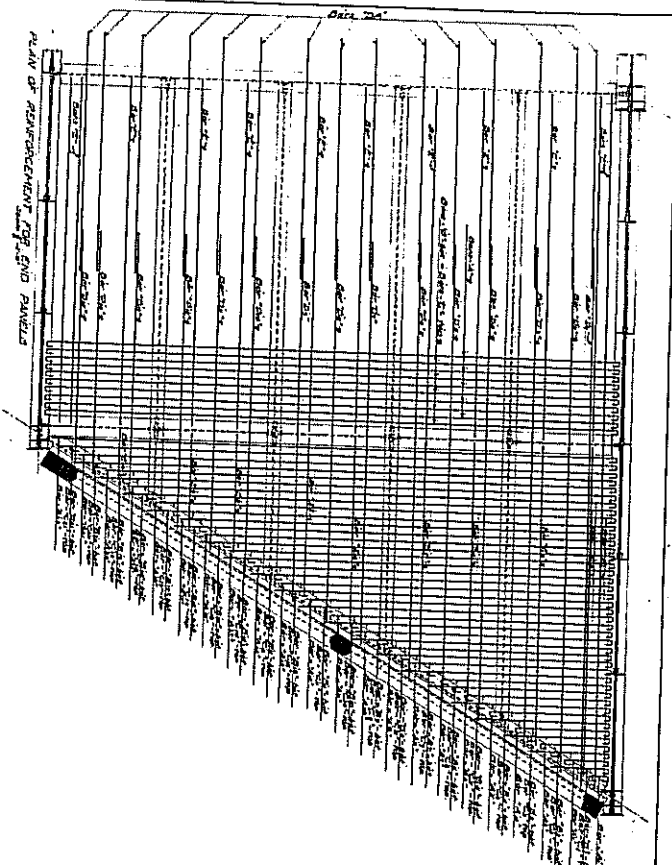
STATE OF NEW YORK OTSEGO COUNTY  
 SHADY SIDE - OTSEGO CH. 317  
 BRIDGE NO. 1 STA. 217+45  
 DETAILS OF SUPERSTRUCTURE

APPROVED

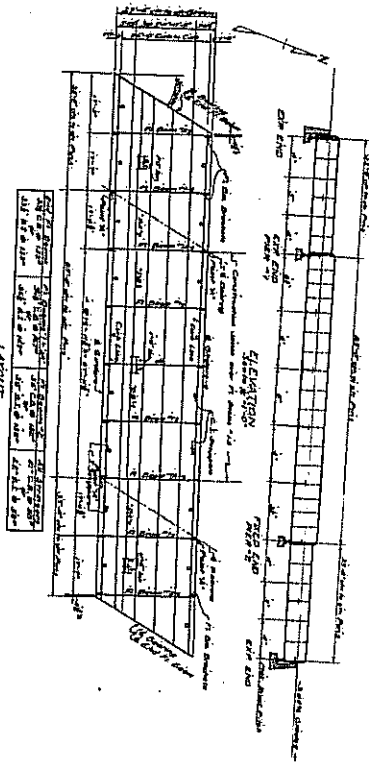
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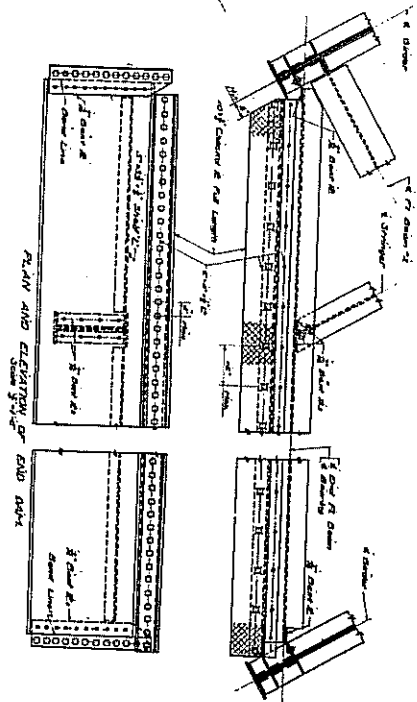
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J. S. MARTIN  
 Chief Engineer  
 New York State Thruway Authority  
 June 1, 1960

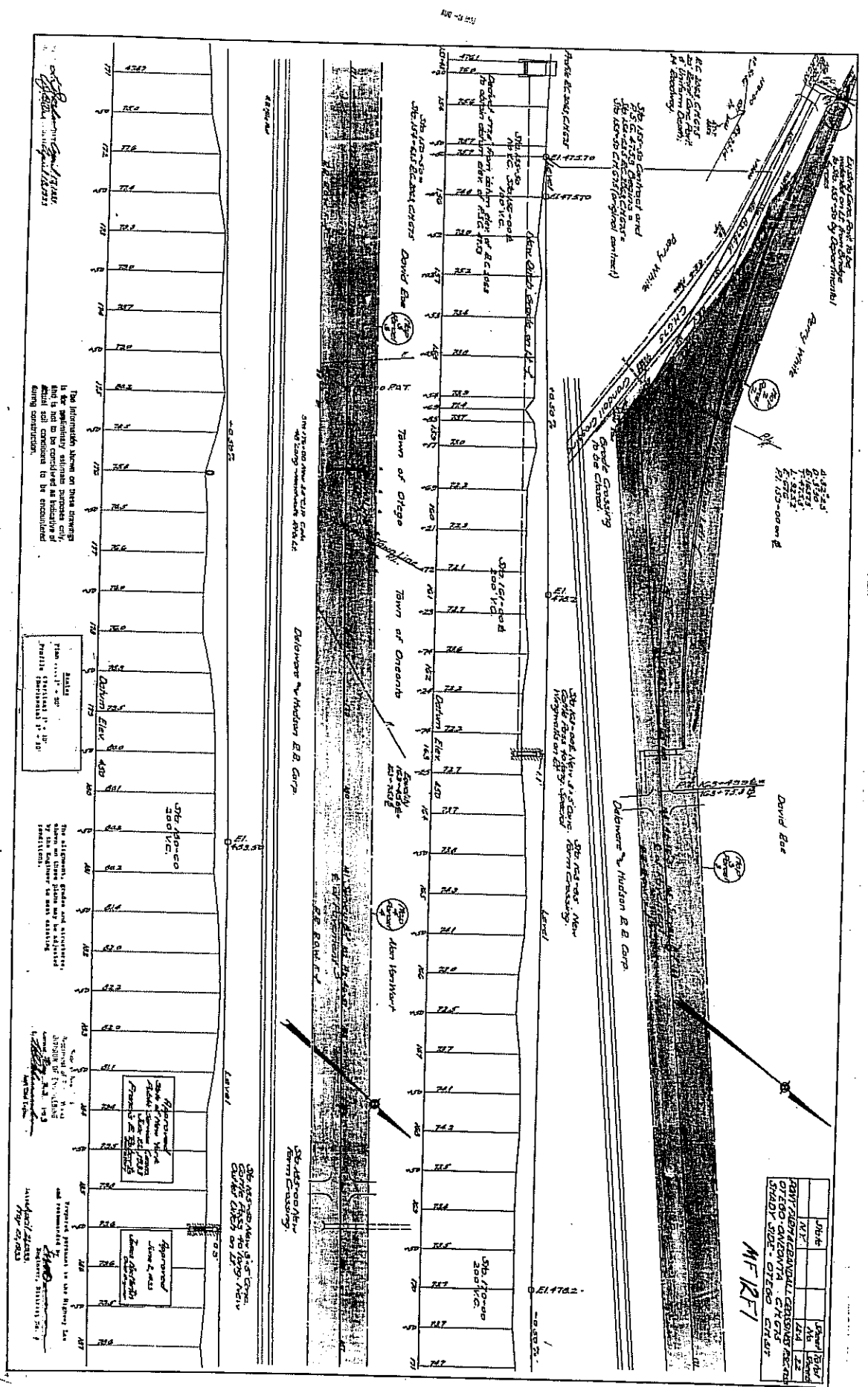


NO.	DESCRIPTION	QTY.	UNIT	REMARKS
1	CONCRETE	1000	CU YD	
2	STEEL REINFORCEMENT	1000	LB	
3	FORMWORK	1000	SQ YD	
4	BRICKWORK	1000	SQ YD	
5	PAINT	1000	GA	
6	GRANITE CURBS	1000	LINEAL FT	
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APPROVED  
 J. S. MARTIN  
 Chief Engineer  
 New York State Thruway Authority  
 STATE OF NEW YORK  
 OTSEGO COUNTY  
 SHADY SIDE - OTEGO  
 BRIDGE NO. 1  
 DETAILS OF SUPERSTRUCTURE  
 STA 27+45  
 C.N. 317

STATE OF NEW YORK  
 COUNTY OF OTSEGO  
 PSC 4763



Job No.	Sheet No.	Scale	Date
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100-1000	110	1" = 10'	10/1/23

MF 1271

Approved by  
*[Signature]*  
 10/1/23

The information shown on these drawings is for preliminary estimate purposes only and is not to be considered as indicative of actual soil conditions to be encountered during construction.

Scale  
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 Profile Horizontal 1" = 20'

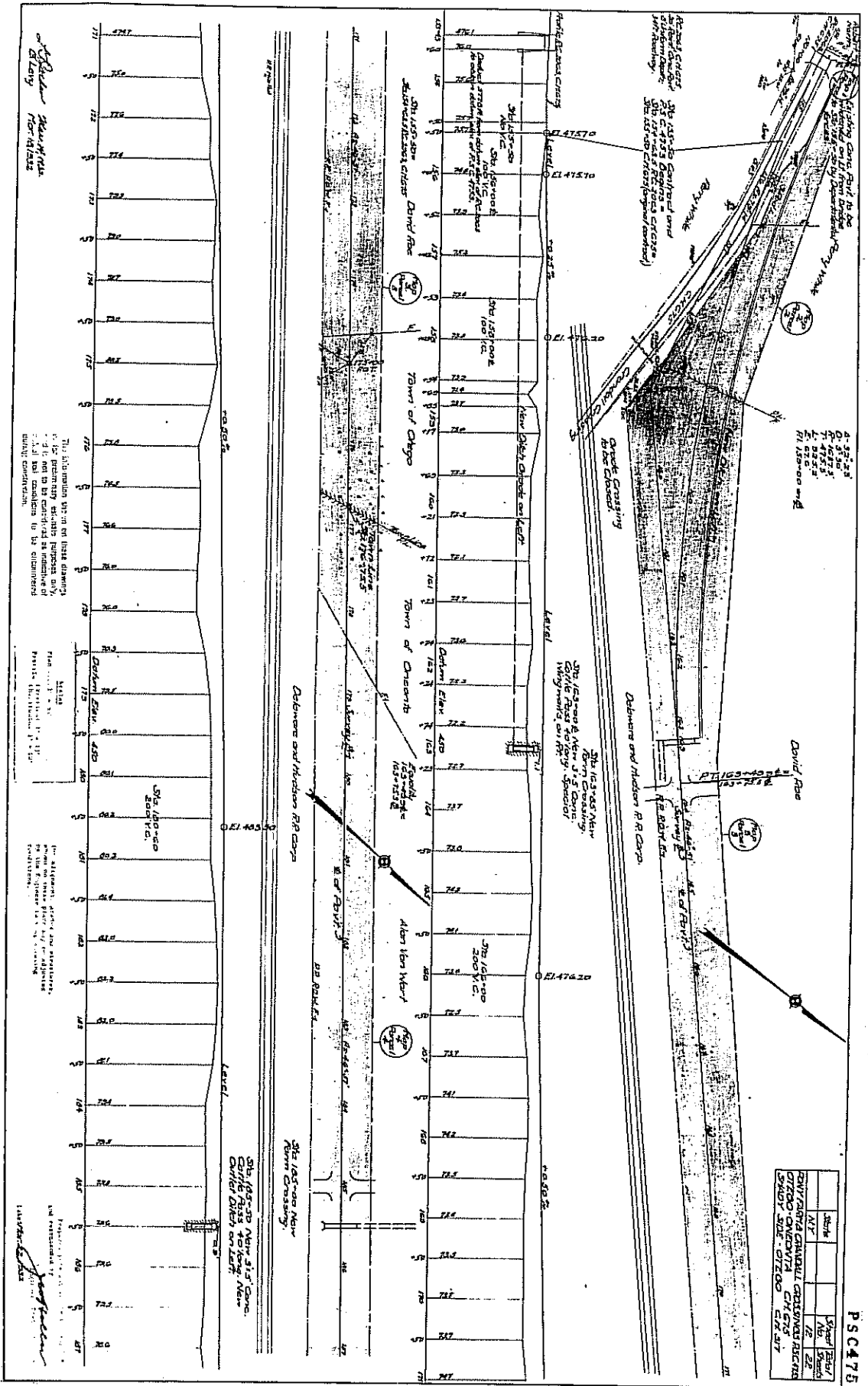
The alignment, grades and structures shown on these plans may be adjusted as needed for each varying condition.

Approved by  
*[Signature]*  
 10/1/23

Vertical curves to be shown on and recommended by  
*[Signature]*  
 10/1/23



1304753



of the  
City

This information is in on these drawings  
is for reference only and should not be  
used as a basis for any construction  
without consultation.

Scale  
1" = 100'

The alignment, grade and structure  
shown on this drawing are for reference  
only and should not be used as a basis  
for any construction.

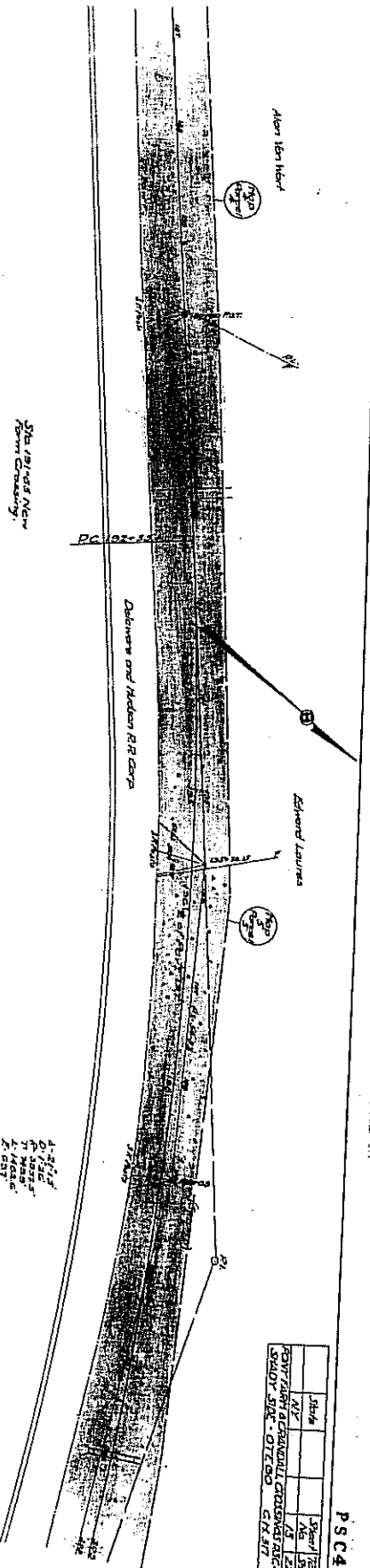
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Checked by  
1/27/2005

PSC4753

1004700

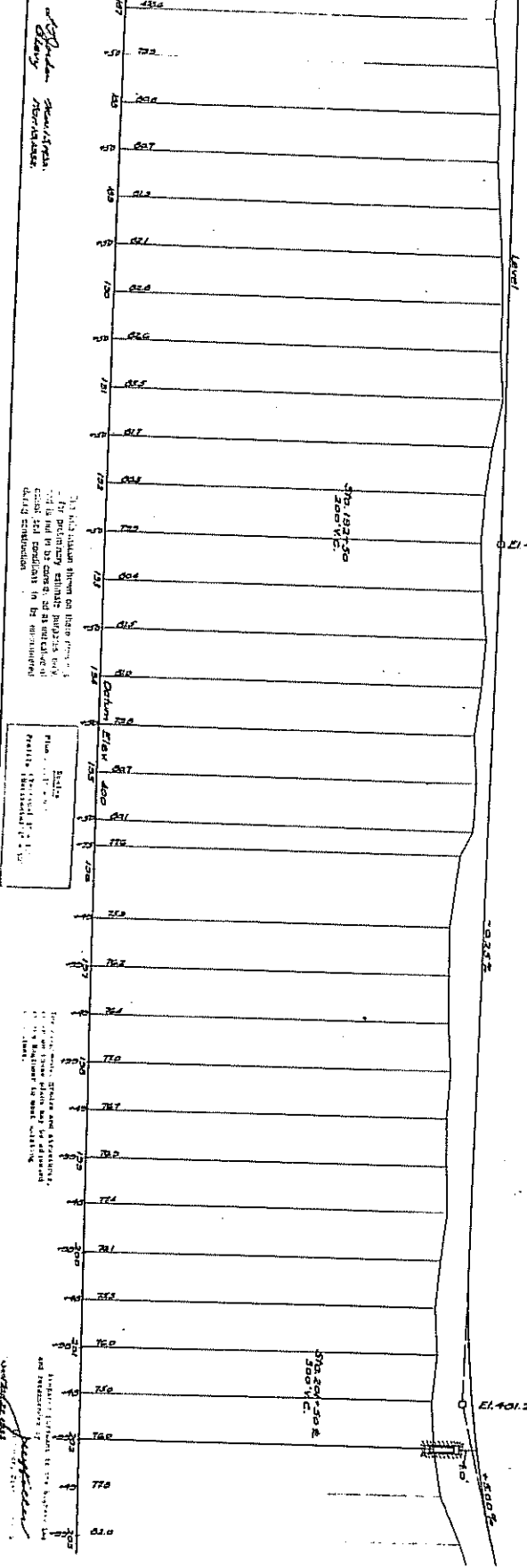
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PSC 4753



4-3-14  
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 4-3-100

Sheets 1-100 of New J.S.F. Plans  
 Civil Engineering



Alton Road  
 New York State  
 Department of Transportation

The following shows the vertical curve data for the proposed road. It is to be used in conjunction with the horizontal curve data and the roadway design data.

Table with 2 columns: Station, Elevation

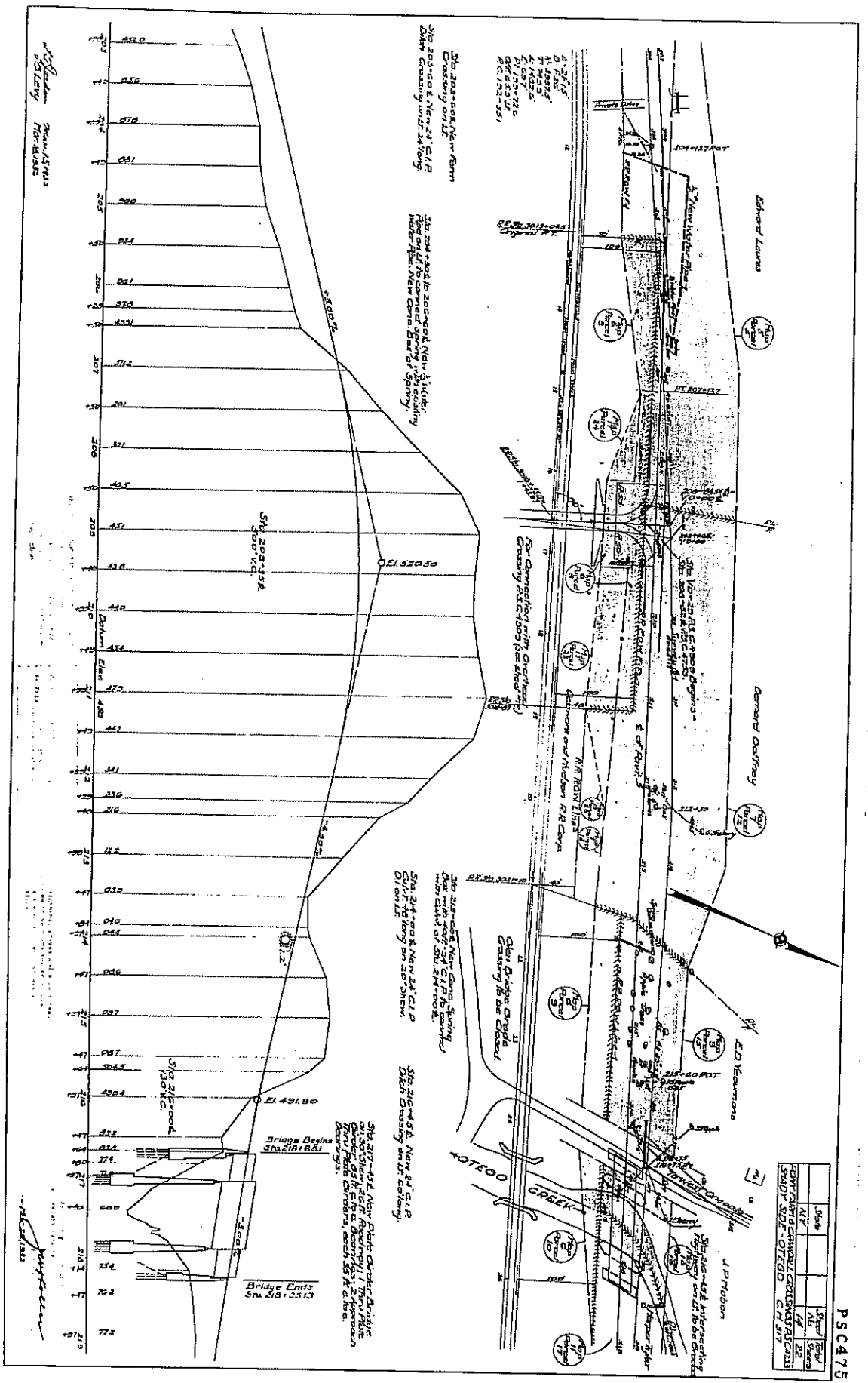
The following shows the vertical curve data for the proposed road. It is to be used in conjunction with the horizontal curve data and the roadway design data.

Sheets 1-100 of New J.S.F. Plans  
 Civil Engineering

1004700

1504793

P5C4753



Station	Plant	Capacity	Notes
218+25.13	Gravel	1000	Existing
218+25.13	Gravel	1000	Existing
218+25.13	Gravel	1000	Existing

Sta. 203+00 to 204+00 New Beam Crossing on L.I.P.

Sta. 204+00 to 206+00 New Lighted Bridge on L.I.P. to connect existing bridge with New Beam Crossing.

Sta. 218+00 to 219+00 New Beam Crossing on L.I.P. to connect existing bridge with New Beam Crossing.

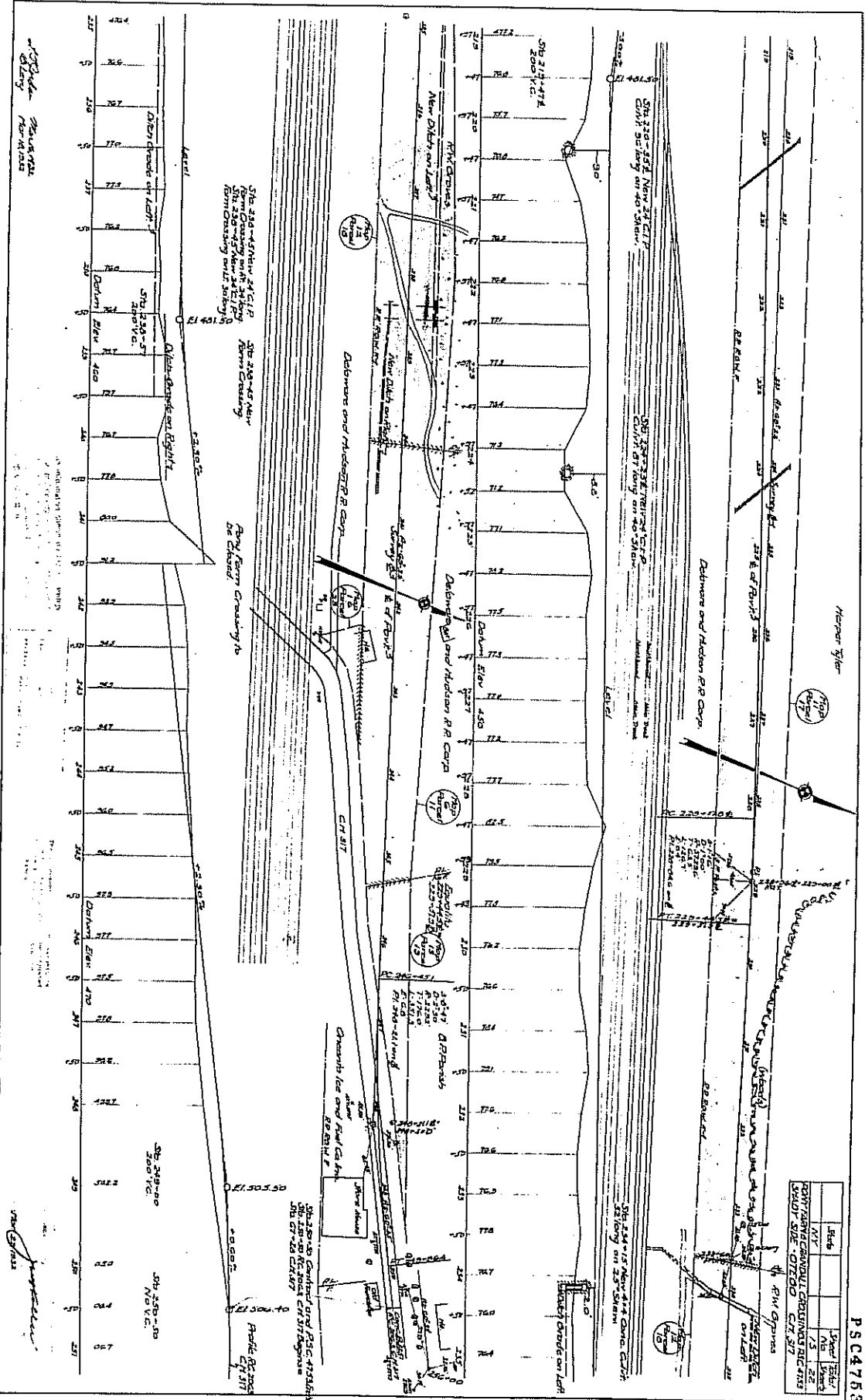
Sta. 219+00 to 220+00 New Beam Crossing on L.I.P.

Sta. 217+50 to 218+00 New Beam Crossing on L.I.P. to connect existing bridge with New Beam Crossing.

Vertical Curve

Horizontal Curve

Sub		Sheet	
REV		NO	DATE
REV		15	22
REV		16	22
REV		17	22
REV		18	22
REV		19	22
REV		20	22
REV		21	22
REV		22	22
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REV		72	22 </tr

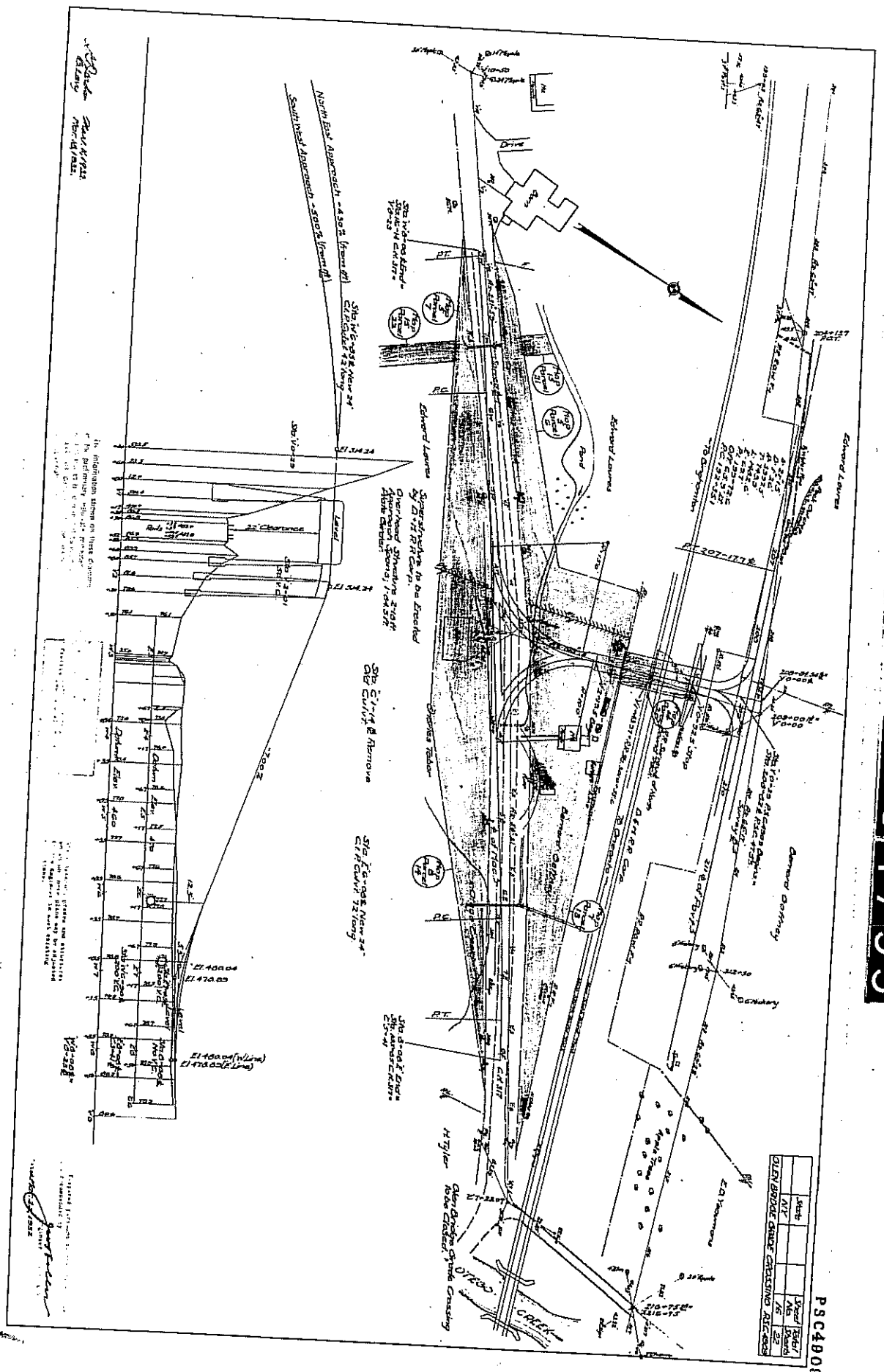


Original Survey Map  
Showing  
New 24' x 48' Garage

Scale  
1" = 20'

PSC4909

Sheet	Sheet	Sheet	Sheet
ATC	ATC	ATC	ATC
ATC	ATC	ATC	ATC



Scale  
1" = 40'  
1/4" = 10'

The information shown on these drawings is for preliminary purposes only. It is not to be used for construction or other purposes without the approval of the engineer.

Not to be used for construction or other purposes without the approval of the engineer.

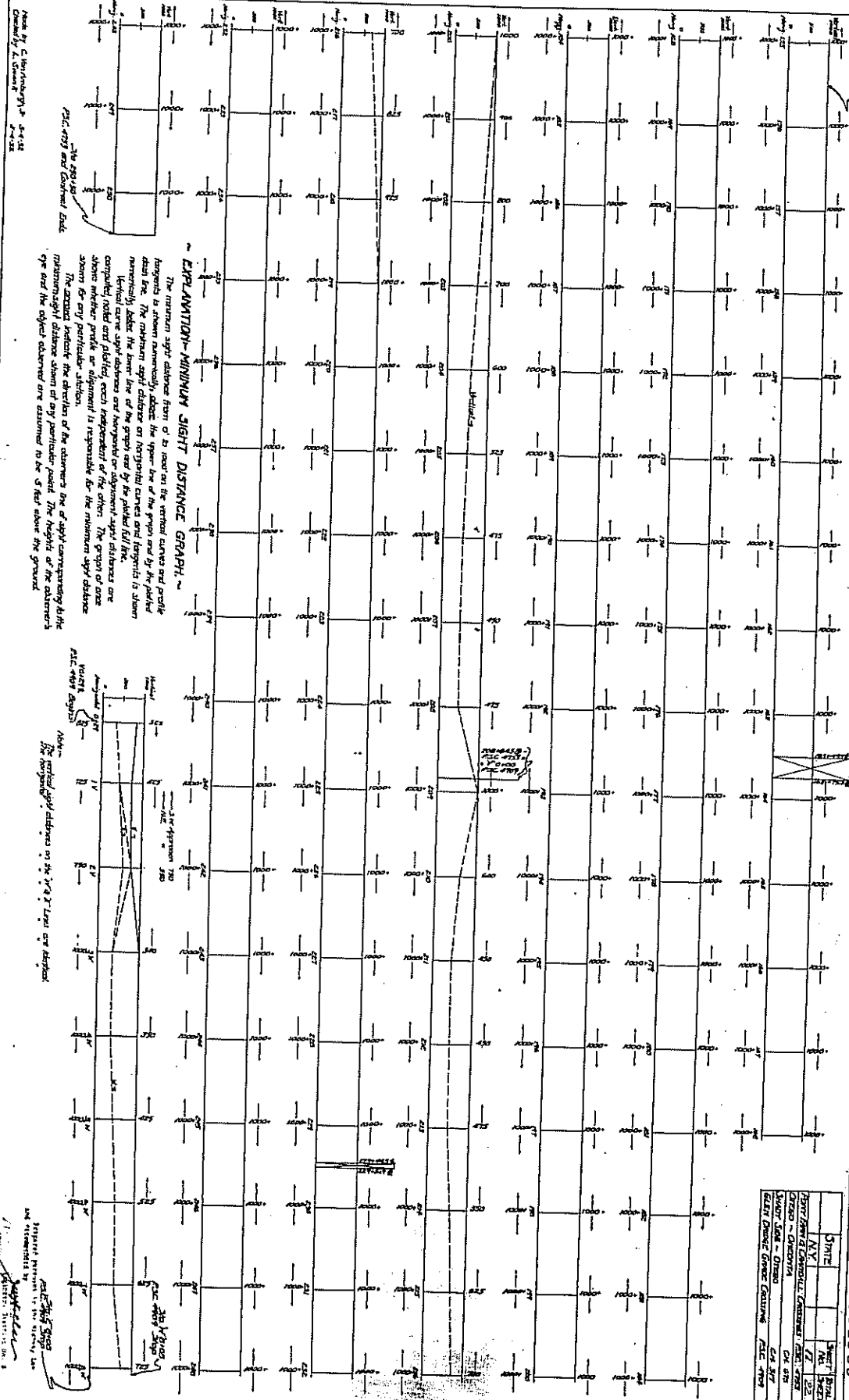
Prepared by  
[Signature]  
[Title]

1004700

1004700

PSC4908

DATE	TIME	STATE	ALT
N.Y.	12:22		
OPERATED BY: [illegible]			
FIELD - [illegible]			
STATION - [illegible]			
ELECTRICIAN - [illegible]			
FIELD - [illegible]			



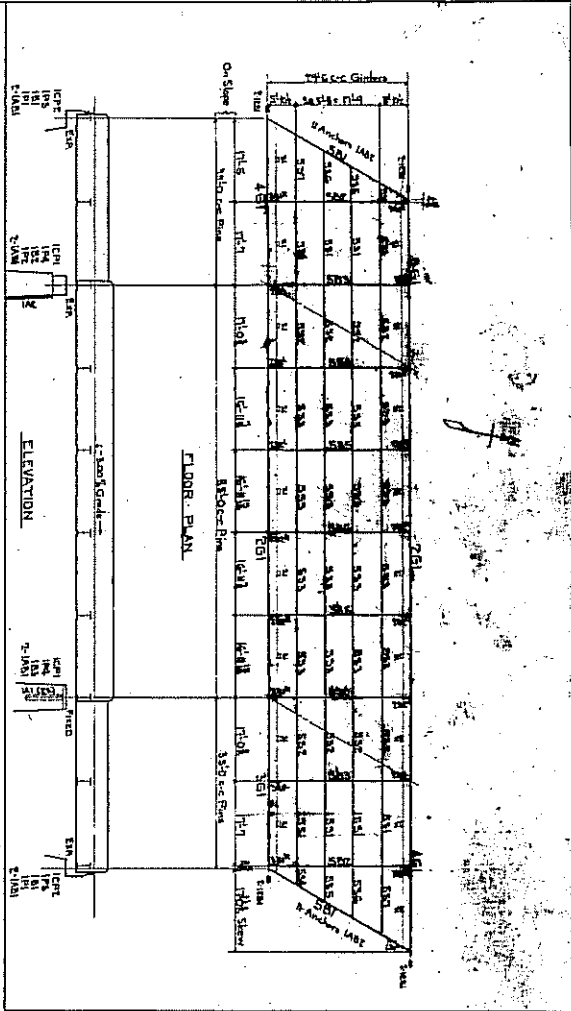
**EXPLANATION - MINIMUM SIGHT DISTANCE GRAPH.**

The minimum sight distance from top of road to the vertical curves and grade diagram is shown numerically above the upper line of the graph and by the plotted horizontal line. The minimum sight distance on the vertical curves and tangents is shown by the lower line of the graph and by the plotted vertical line. The minimum sight distance on the vertical curves and tangents is shown by the lower line of the graph and by the plotted vertical line. The minimum sight distance on the vertical curves and tangents is shown by the lower line of the graph and by the plotted vertical line.

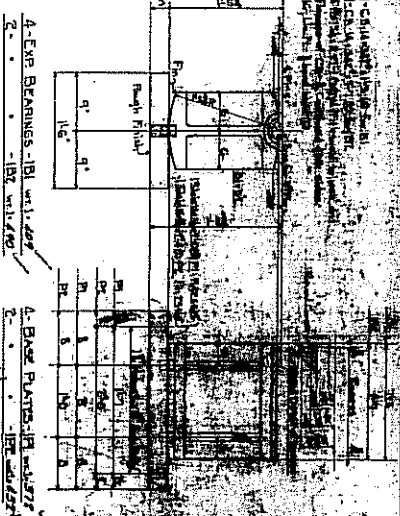
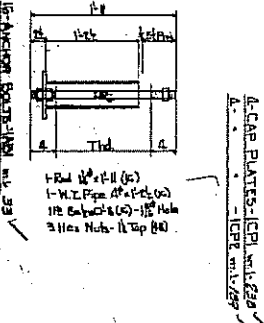
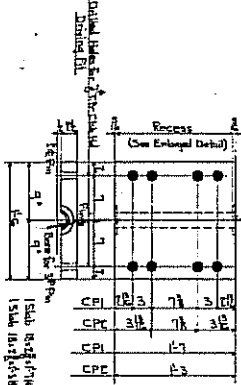
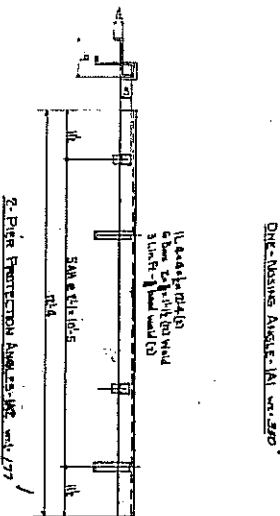
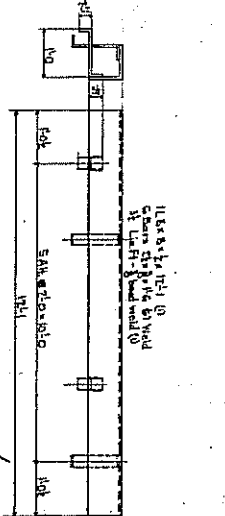
Made by C. Hartmann, Jr. 3-1-33  
Checked by A. Stewart 3-1-33

Approved by: [Signature]  
Checked by: [Signature]

PSC 4753

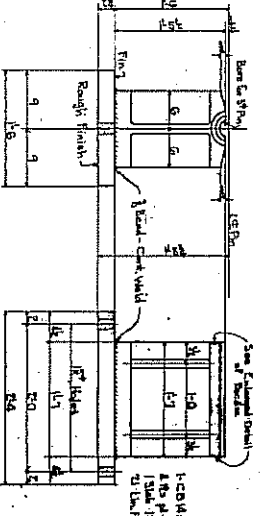


FLOOR PLAN  
ELEVATION

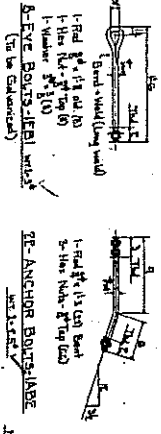


4. EXP BEARINGS - 1B1 with 4/8  
- 1B2 with 4/8

4. BASE PLATES - 1B1 with 4/8  
- 1B2 with 4/8

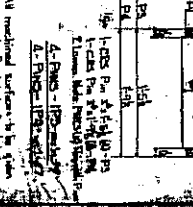
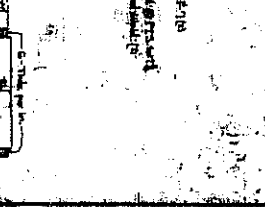


2. FIXED BEARINGS - 1B3 with 1/2"



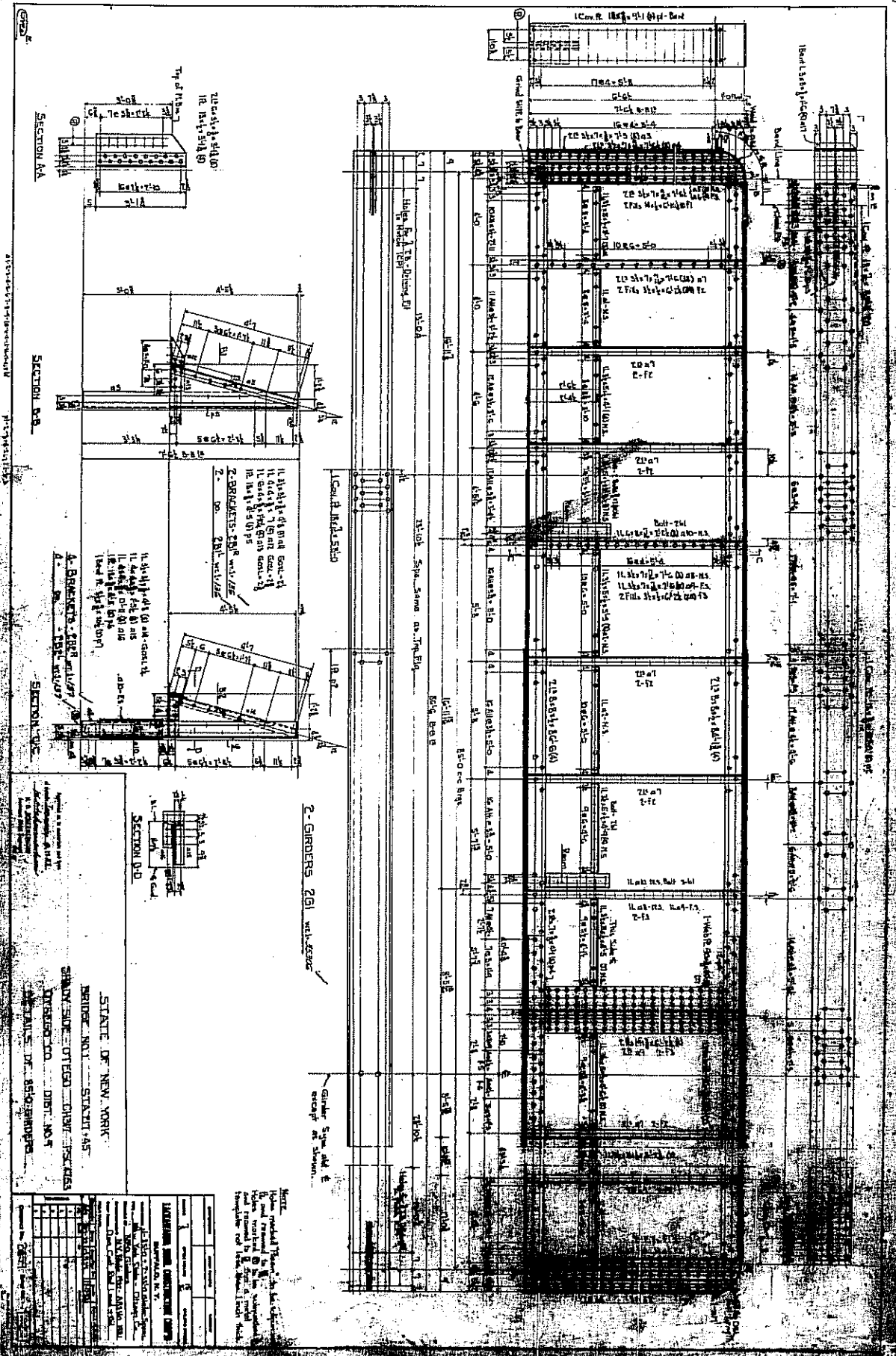
1B - CT SQUARES - SI with 1/2"

2E - ANCHOR BOLTS - 1/2"



STATE OF NEW YORK  
BRIDGE NO. 1 - STA 118+5  
BRIDGE SIDE - DIRECTION - CHART - 118+5  
DRAWN BY: [Name]  
CHECKED BY: [Name]  
DATE: [Date]

NO.	REVISION	DATE	BY	CHECKED
1	AS SHOWN			
2	REVISION			
3	REVISION			
4	REVISION			



SECTION AA

SECTION BB

SECTION CC

SECTION DD

STATE OF NEW YORK  
 BRIDGE NO. 1 STARK 45  
 SHAWNEE OVEEG DRAW 1000  
 DRAWING NO. 1  
 STATE OF NEW YORK

NOTE

1. All steel work to be fabricated in accordance with the specifications of the American Institute of Steel Construction, Inc., 1918 Edition, and the specifications of the American Bridge Company, Inc., 1918 Edition.

2. All steel work to be painted with a heavy coat of red lead paint.

3. All steel work to be galvanized.

4. All steel work to be fireproofed.

5. All steel work to be bolted with high tensile bolts.

6. All steel work to be riveted with high tensile rivets.

7. All steel work to be welded with high tensile weld metal.

8. All steel work to be drilled with high speed drill.

9. All steel work to be cut with oxy-acetylene.

10. All steel work to be bent with a hot iron.

11. All steel work to be straightened with a hot iron.

12. All steel work to be finished with a smooth surface.

13. All steel work to be free from scale and rust.

14. All steel work to be free from weld spatter.

15. All steel work to be free from sharp corners.

16. All steel work to be free from burrs.

17. All steel work to be free from sharp edges.

18. All steel work to be free from sharp points.

19. All steel work to be free from sharp projections.

20. All steel work to be free from sharp indentations.

21. All steel work to be free from sharp depressions.

22. All steel work to be free from sharp notches.

23. All steel work to be free from sharp grooves.

24. All steel work to be free from sharp slots.

25. All steel work to be free from sharp holes.

26. All steel work to be free from sharp openings.

27. All steel work to be free from sharp projections.

28. All steel work to be free from sharp indentations.

29. All steel work to be free from sharp depressions.

30. All steel work to be free from sharp notches.

31. All steel work to be free from sharp grooves.

32. All steel work to be free from sharp slots.

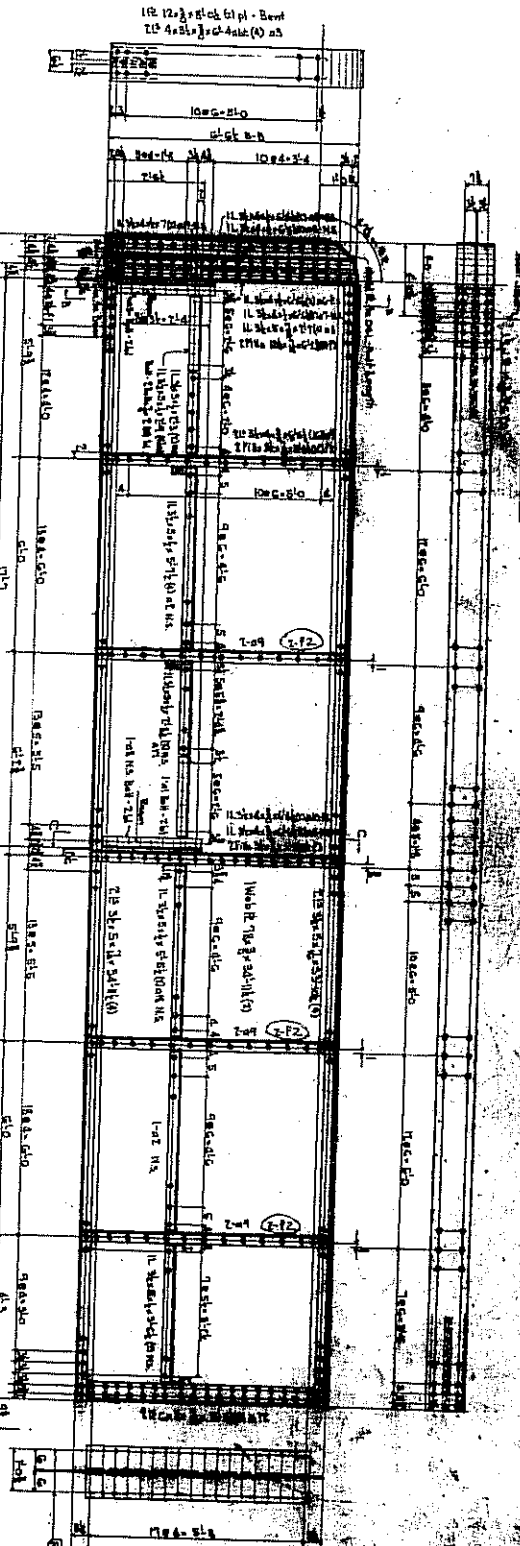
33. All steel work to be free from sharp holes.

34. All steel work to be free from sharp openings.

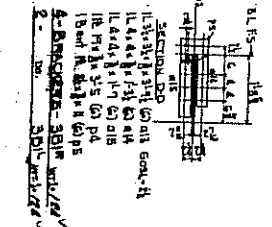
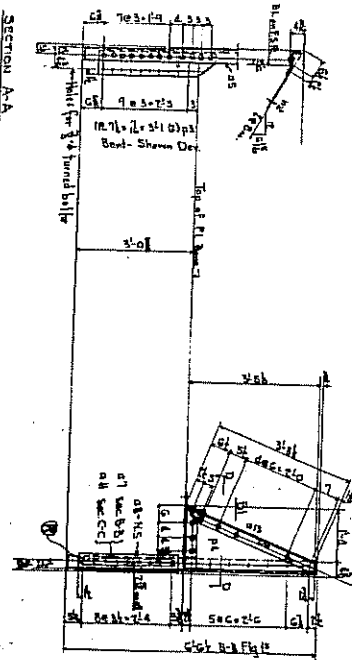
2 - GIRDERS 261 well-steel

Girders 261 well-steel & except as shown.





2-GIRDERS-361-Wt. 2770



SECTION B-B  
SECTION - 210 (See above)

STATE OF NEW YORK  
BRIDGE NO. 1 STA. 21+45  
SHADY SPRING BRIDGE CIVIL PROJECTS  
DESIGNED BY: [unclear]  
DATE: [unclear]

**NOTE**  
These drawings were prepared for the purpose of providing information only. They are not to be used for construction of any structure without the approval of the Engineer of Record. The Engineer of Record is not responsible for any errors or omissions in these drawings or for any consequences arising therefrom.

WORK TOGETHER SH. S/D

NO.	DATE	DESCRIPTION
1	10/25/45	PRELIMINARY
2	11/10/45	REVISED
3	11/15/45	REVISED

APPROVED FOR THE STATE OF NEW YORK  
BY: [unclear]  
DATE: [unclear]

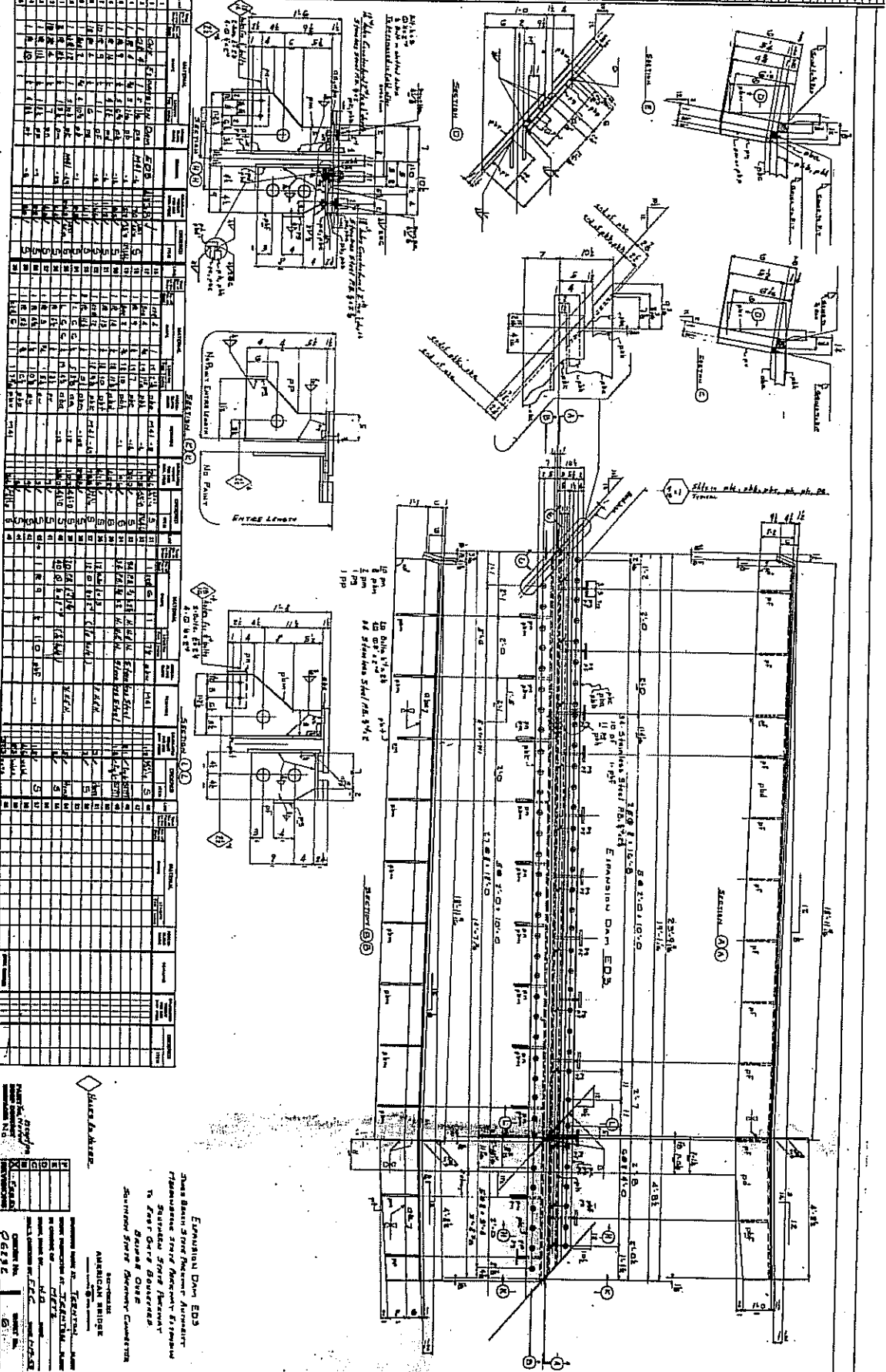
DESIGNED BY: [unclear]  
DATE: [unclear]

PROJECT NO. 6451





NO.	DESCRIPTION	DATE	BY	CHECKED



**EXPANSION DAM EDS**  
 DAM BUILT BY THE FEDERAL GOVERNMENT  
 FOR THE STATE OF CALIFORNIA  
 IN THE COUNTY OF SACRAMENTO  
 AND THE COUNTY OF BUTTE  
 UNDER THE SUPERVISION OF THE  
 ARCHITECTURAL ENGINEER  
 AND THE CIVIL ENGINEER  
 IN CHARGE OF THE PROJECT  
 CONTRACT NO. EDC-100  
 DRAWING NO. 01

Discovery Date: 12/12/2007

RC: 94 BIN: 2227870

**RED Flag 9C070093**

Carried: PONY FARM ROAD    Crossed: DELAWARE & HUDSON

Prompt Interim Action Recommended: No

Inspector: Henderson, Ryan  
Flag Number: 9C070093

Date Discovered: 12/12/2007  
Supersedes Flag Number: 9W060016

Bridge Description:

BIN: 2227870    Carried: PONY FARM ROAD    Crossed: DELAWARE & HUDSON

Region: 9 - Binghamton    County: 4 - Otsego  
Political Unit: 0610 - Town of Oneonta  
Primary Owner: 60 - Railroad  
Secondary Owner: 99 - One Agency - Listed in first subfield  
Primary Maintenance: 40 - Town  
Secondary Maintenance: 60 - Railroad  
Year Built: 1933    Posted For Load: 15 Tons

Number of Spans by Type:	Num	Type	Description
	001	- 115	- Steel - Plate Girder-Floorbeam System, Thru
	002	- B01	- Concrete, Reinforced - Slab

Description of Flagged Condition:

RED STRUCTURAL FLAG 9C070093 ISSUED (Flag 9W060016 superseded):

The SPAN 2 LEFT Pedestal exhibits crumbly concrete, with a fracture line extending through the Begin ("rear") anchor bolts of the Bearing reaching a maximum length of 5 ft. down the side of the Pier Column with thick efflorescence extending another 1 ft. The entire Pedestal surface is spalled up to 8 in. deep, with crumbly concrete extending at up to 1 in. into the remaining material. Cleaning of loose material reveals a line of deeper deterioration, with a crack evident through both cement paste and aggregate. The crack appears to be a product of general deterioration rather than active shear failure of the concrete. But it forms a plane of weakness which effectively undermines 95% of the Bearing. No significant change to this condition as well as no sign of movement or settlement were observed this inspection. Vertical rebars with minor section losses were noted in the column/pedestal concrete.

The SPAN 2 RIGHT Pedestal exhibits crumbly concrete, with undermining along the front face of the Bearing up to 2.5 in. for Span 3 G2. The undermining reduces the bearing area by approximately 14%, which is a slight increase from the previous inspection (12%). The Pedestal is spalled up to 6 in. deep, with up to an additional 1 in. depth of crumbly concrete. The Right Pedestal has been included in this flag to show the condition of the overall load transferring system of superstructure to the substructure.

The Pedestals and Bearings support Load-path Non-redundant Girders. The advanced deterioration limits the service life of the Pedestals. This condition has been noted in the last inspection with minor change in deterioration since then.

Upon QC review it was determined that based on the active deterioration of the pedestal concrete and the fact that the pedestal supports a non-redundant thru-girder, a RED FLAG is warranted.

2 Photos/Sketches Attached

Verbal Notifications: (For RED Flags and Safety Flags with PIA only)

**Discovery Date: 12/12/2007**

**RC: 94 BIN: 2227870**

**RED Flag 9C070093**

**Carried: PONY FARM ROAD Crossed: DELAWARE & HUDSON**

To: Tim Giblin of Regional Office on 12/19/2007 at 12:45:00 PM

Signature: (a signed copy of this report will be placed in the BIN folder)

Flagged Bridge Report Completed By: Henderson, Ryan on 12/18/2007

Flagged Bridge Report Signed By: \_\_\_\_\_ on \_\_\_\_\_  
Henderson, Ryan

(This PDF Report Created: 12/19/2007 12:49:34 PM)

Discovery Date: 12/12/2007

RC: 94 BIN: 2227870

**RED Flag 9C070093 Attachment**

Carried: PONY FARM ROAD    Crossed: DELAWARE & HUDSON

07-1476.jpg - Attached to RED Flag 9C070093

Left End Side of Right Column of Pier 2



Discovery Date: 12/12/2007

RC: 94 BIN: 2227870

RED Flag 9C070093 Attachment

Carried: PONY FARM ROAD    Crossed: DELAWARE & HUDSON

07-1480.jpg - Attached to RED Flag 9C070093

Left Side of Left Column of Pier 2





Discovery Date: 12/12/2007

RC: 94 BIN: 2227870

**RED Flag 9C070094**

Carried: PONY FARM ROAD    Crossed: DELAWARE & HUDSON

Prompt Interim Action Recommended: No

Inspector: Henderson, Ryan  
Flag Number: 9C070094

Date Discovered: 12/12/2007  
Supersedes Flag Number: 9W060017

Bridge Description:

BIN: 2227870    Carried: PONY FARM ROAD    Crossed: DELAWARE & HUDSON

Region: 9 - Binghamton    County: 4 - Otsego  
Political Unit: 0610 - Town of Oneonta  
Primary Owner: 60 - Railroad  
Secondary Owner: 99 - One Agency - Listed in first subfield  
Primary Maintenance: 40 - Town  
Secondary Maintenance: 60 - Railroad  
Year Built: 1933    Posted For Load: 15 Tons

Number of Spans by Type:	Num	Type	Description
	001	- 115	- Steel - Plate Girder-Floorbeam System, Thru
	002	- B01	- Concrete, Reinforced - Slab

Description of Flagged Condition:

RED STRUCTURAL FLAG 9C070094 Issued (Flag 9W060017 Superseded):

In Span 3, the bottom flanges of all Floorbeams exhibit heavy rust scale with section losses at their centers. Losses are estimated based a combination of caliper readings of exposed flange edges, pitting depths, and visual comparisons, as follows:

FB0-20%;    FB1-30%;    FB2-25%;    FB3-42%;    FB4-35%;    FB5-35%;    FB6-25%

Floorbeams are classified Load Path Non-Redundant at 14'-1" spacing. Webs and top flanges are encased and likely to have only minimal losses. Some slight variations in losses were observed from the previous inspection (however not to FB3, which should be the controlling member), but no distress or excess deformation is evident.

Upon QC review it was determined that the condition of the Floorbeams warrants a RED Flag. Yellow Flags have been issued in the past two inspections for the same condition.

It should be noted that the actual member properties are unavailable due to lack of record plans and the concrete blast protection which covers the majority of the beams. The section losses are based on the assumption that the flanges have a uniform thickness of 0.75" (also used in BLRS analysis).

1 Photos/Sketches Attached

Verbal Notifications: (For RED Flags and Safety Flags with PIA only)

To: Tim Giblin of Regional Office on 12/19/2007 at 12:45:00 PM

Signature: (a signed copy of this report will be placed in the BIN folder)

Flagged Bridge Report Completed By: Henderson, Ryan on 12/18/2007

Flagged Bridge Report Signed By: \_\_\_\_\_ on \_\_\_\_\_  
Henderson, Ryan

**Discovery Date: 12/12/2007**

**RC: 94 BIN: 2227870**

**RED Flag 9C070094**

**Carried: PONY FARM ROAD    Crossed: DELAWARE & HUDSON**

(This PDF Report Created: 12/19/2007 12:49:37 PM)

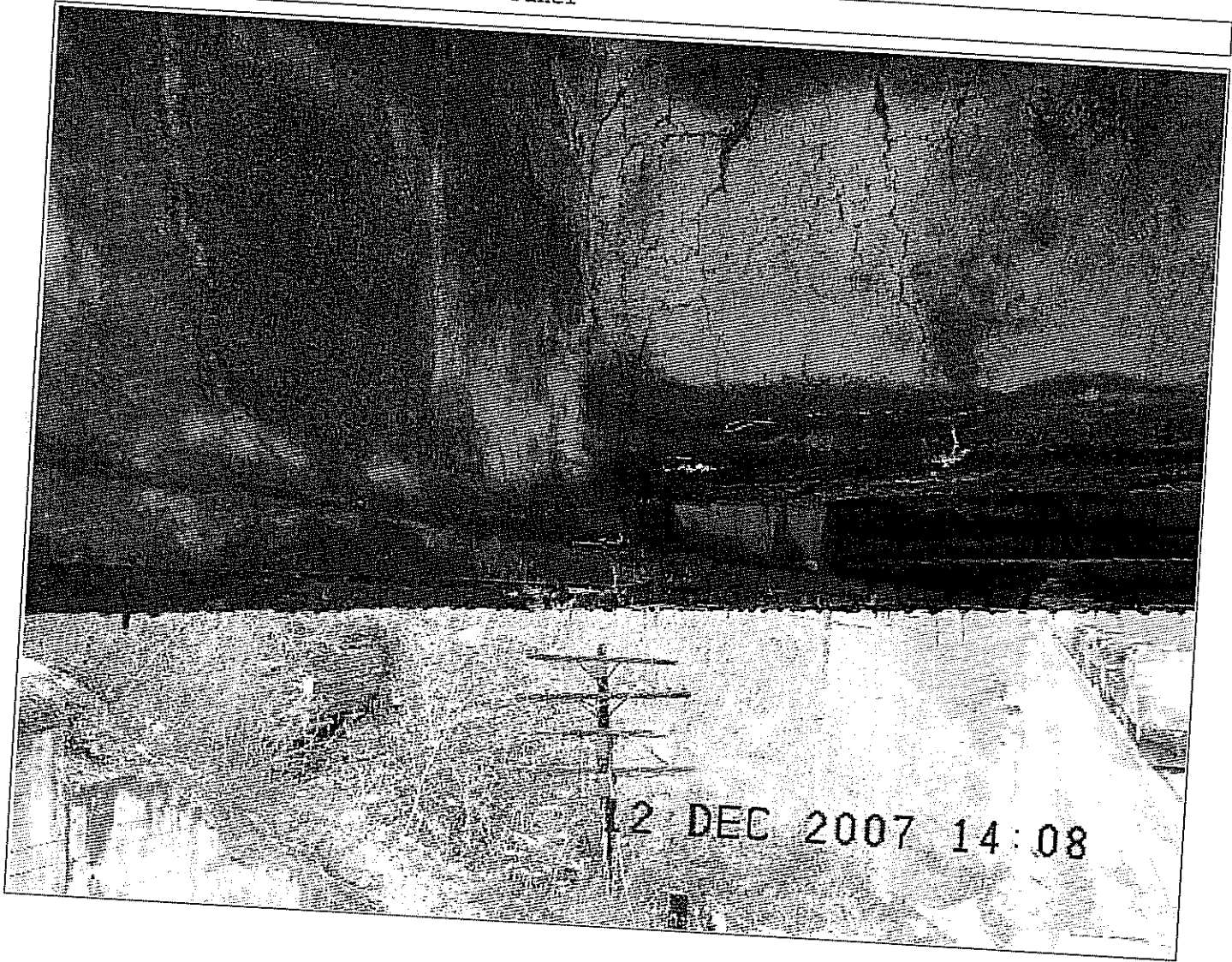
Discovery Date: 12/12/2007

RC: 94 BIN: 2227870

RED Flag 9C070094 Attachment

Carried: PONY FARM ROAD    Crossed: DELAWARE & HUDSON

07-1484.jpg - Attached to RED Flag 9C070094  
Begin Face of Floorbeam 4 at Mid-Panel



Discovery Date: 12/12/2007

RC: 94 BIN: 2227870

**RED Flag 9C070095**

Carried: PONY FARM ROAD    Crossed: DELAWARE & HUDSON

Prompt Interim Action Recommended: No

Inspector: Henderson, Ryan  
Flag Number: 9C070095

Date Discovered: 12/12/2007  
Supersedes Flag Number: \_\_\_\_\_

Bridge Description:

BIN: 2227870    Carried: PONY FARM ROAD    Crossed: DELAWARE & HUDSON

Region: 9 - Binghamton    County: 4 - Otsego  
Political Unit: 0610 - Town of Oneonta  
Primary Owner: 60 - Railroad  
Secondary Owner: 99 - One Agency - Listed in first subfield  
Primary Maintenance: 40 - Town  
Secondary Maintenance: 60 - Railroad  
Year Built: 1933    Posted For Load: 15 Tons

Number of Spans by Type:	Num	Type	Description
	001	- 115	- Steel - Plate Girder-Floorbeam System, Thru
	002	- B01	- Concrete, Reinforced - Slab

Description of Flagged Condition:

RED STRUCTURAL FLAG 9C070095 Issued (No Previous Flag Superseded):

In Span 3, the bottom flanges of almost all Stringers exhibit heavy rust scale with section losses at their centers. Losses are estimated based a combination of caliper readings of exposed flange edges, pitting depths, and visual comparisons, as follows:

Panel 1 -	S1: 20%	S2: 20%	S3: 10%
Panel 2 -	S1: 20%	S2: 20%	S3: 15%
Panel 3 -	S1: 30%	S2: 40%	S3: 20%
Panel 4 -	S1: 35%	S2: 45%	S3: 30%
Panel 5 -	S1: 30%	S2: 20%	S3: 15%
Panel 6 -	Minimal Losses.		

Stringers are classified Load Path Redundant. Webs and top flanges are encased and likely to have only minimal losses. Some slight variations in losses were observed, but no distress or excess deformation is evident.

Upon QC review it was determined that the condition of the Stringers warrants a RED Flag, as it represents > 1/3 flange loss to adjacent redundant members.

It should be noted that the actual member properties are unavailable due to lack of record plans and the concrete blast protection which covers the majority of the beams. The section losses are based on a conservative assumption that the flanges have a uniform thickness of 0.5" (thickness used in BLRS analysis).

2 Photos/Sketches Attached

Verbal Notifications: (For RED Flags and Safety Flags with PIA only)

To: Tim Giblin of Regional Office on 12/19/2007 at 12:45:00 PM

Signature: (a signed copy of this report will be placed in the BIN folder)  
Flagged Bridge Report Completed By: Henderson, Ryan on 12/18/2007



Discovery Date: 12/12/2007

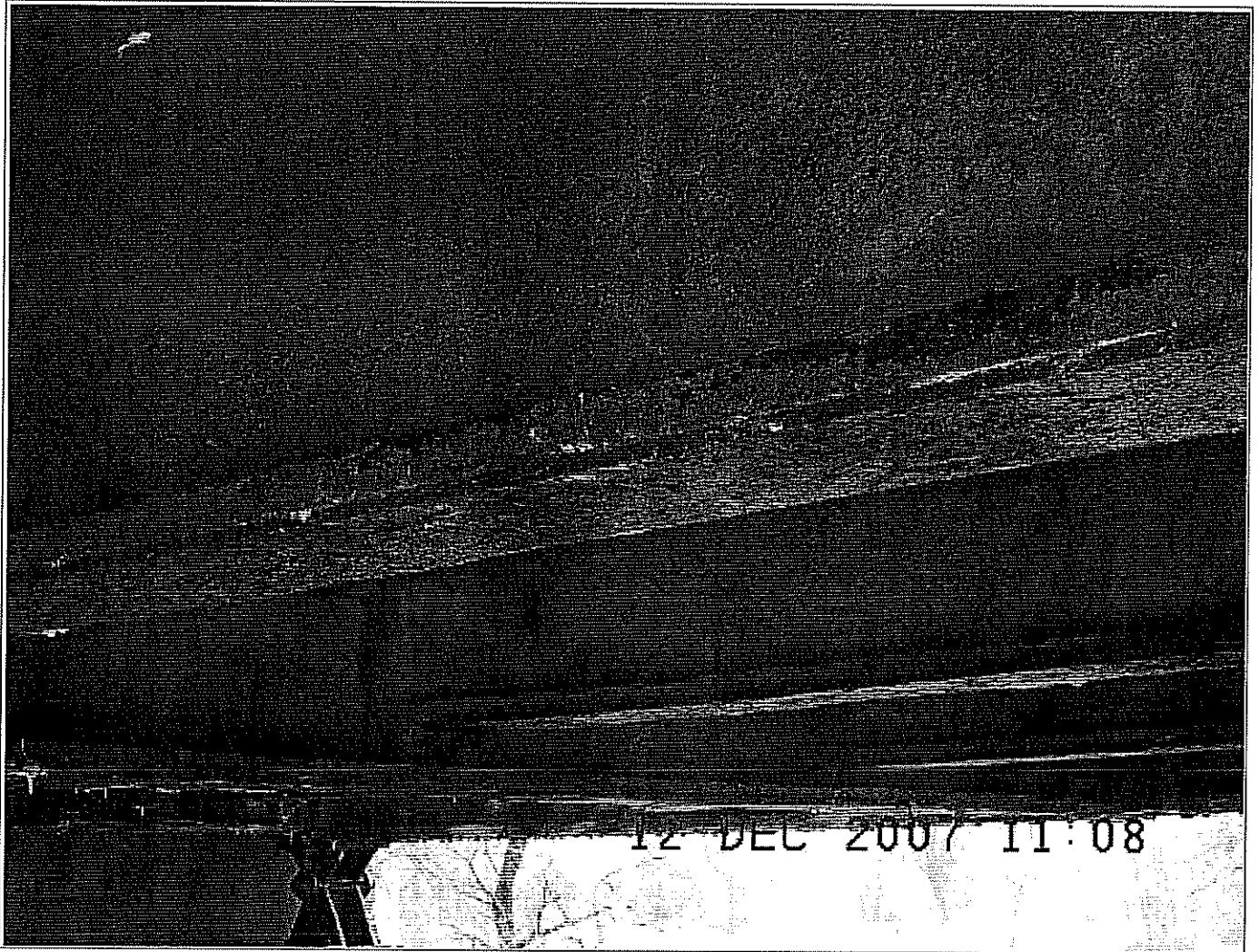
RC: 94 BIN: 2227870

RED Flag 9C070095 Attachment

Carried: PONY FARM ROAD    Crossed: DELAWARE & HUDSON

07-1475.jpg - Attached to RED Flag 9C070095

Panel 2 Stringer 3, Left Side



Discovery Date: 12/12/2007

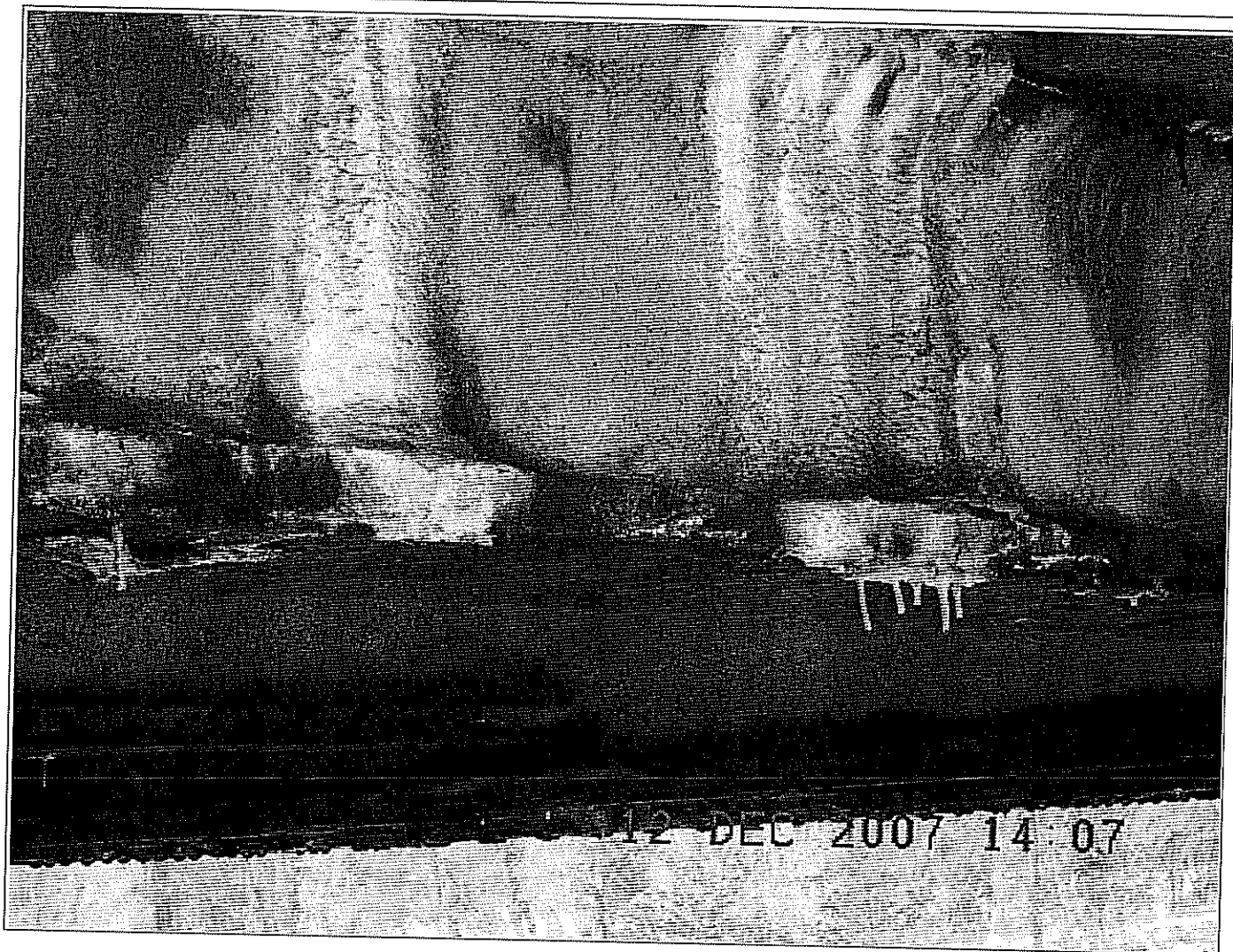
RC: 94 BIN: 2227870

RED Flag 9C070095 Attachment

Carried: PONY FARM ROAD    Crossed: DELAWARE & HUDSON

07-1483.jpg - Attached to RED Flag 9C070095

Panel 4 Stringer 2, Left Side



# The Daily Star The Newspaper for

Wednesday, October 11, 2006

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## News

9-27-2006



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Local News

## 2 bridges will be replaced

- Local News

Archives

By Jake Palmateer

Local Sports

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Archives

Staff Writer

Obituaries

- Obituary

ONEONTA \_ Two area bridges are being replaced under an agreement between the state and Canadian Pacific Railway, state and railroad officials announced Monday.

Archives

Local Opinion

Local Lifestyle

Community News

Police Blotter

The state will provide \$1.1 million for a Brooker Hollow Road bridge in East Worcester and a bridge between state Route 7 and Pony Farm Road in the town of Oneonta. Both bridges span CP Rail's tracks, officials said.

Ap Wire

Business

## Marketplace

Autos

CP Rail will be picking up roughly half of the estimated \$2.1 million total cost to replace both bridges, according to state Sen. James Seward, R-Milford. Seward, along with Stephen Fisk of CP Rail, Oneonta Town Supervisor Duncan Davie and Worcester Councilwoman Lynda Clark, made the announcement at the Oneonta Town Hall on Monday.

Homes

Thresholds

Apartments

Jobs

- Heartland Job Search

There would be no local costs associated with the two projects, Seward said.

- Employment Opportunities

Retail

Fisk, a senior manager of business development for the railroad, said preliminary engineering work would begin this winter with construction "hopefully" completed by the fall of next year.

Yellow Pages

All

## Photos

Reader Photo

"I don't want to make any promises I can't keep," Fisk said.

Galleries

Historical Heartland

Fisk said the timetable still needs to be developed, adding that there is no indication of how long the Brooker Hollow Road bridge, which officials said was their most immediate concern, would be out during the construction.

Purchase Photo

Reprints

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Requests

The previous detour, which was in place for two months last winter, meant an 18-mile commute over "treacherous roads," Clark said.

- Order Full Page

Reprints

## Other

School buses were forced to run an extra 100 miles daily to detour when



**Features**

- Community Calendar
- Cooperstown Crier
- Weather
- Newspapers in Education

the bridge was closed, according to school officials.

There were also concerns about response times for emergency services

"I know the people in East Worcester are going to be thrilled," Clark said.

The bridge was closed last November after the state Department of Transportation found it deficient, but it was reopened in mid-January on a temporary basis.

Advertisements



Cheryl Irvine, a 28-year resident of Brooker Hollow Road, said Monday she was pleased a deal between the state and the railway was reached.

LICENSED FUNERAL DIRECTORS

"I think it's great they were able to work out some kind of concerted effort on this," Irvine said.

**AUTO**



However, Irvine said she was still skeptical of how the replacement will eventually pan out, especially when it comes to the timing of the work, length of a bridge closure and the condition of the detour.

"There are public-safety issues with this," Irvine said. "The (detour) road is in horrendous shape."



To ensure public safety, Irvine said, Brooker Hollow Road would need to be reopened before winter sets in.

Town & Country Lawn Care

The detour went over a steep hill, she said.



Clark said the town of Worcester has previously spent about \$20,000 in costs related to the bridge, including improvements along the detour route.



Because state funds are involved, the project is subject to a public-bidding process, Fisk said.

The projects may be put out to bid as a package, Fisk said.

He added that availability of steel could be one factor that could delay construction.

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The state funds were allocated in the state budget for capital projects, according to Seward.

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Monday, January

12/28/05

## Official: Bridge near fix

By Mark Boshnack

Staff Writer

Progress is being made on a temporary fix that could result in an East Worcester bridge being reopened in the first half of January, a town official said Tuesday.

The Brooker Hollow Road bridge was closed in early November after a state Department of Transportation inspection indicated potential failure of a critical primary structure.

A response was delayed as the town and the Delaware and Hudson CP Rail System argued about ownership.

At a meeting with state officials in early December, both sides agreed to split the cost of the work, estimated to be \$25,000 to \$35,000, without claiming ownership.

The blueprints for the project were received from Clough Harbor Associates of Albany on Friday, Worcester Town Supervisor Christopher Clark said Tuesday.

The town is finishing plans to hire a contractor for the repairs needed to reopen the bridge to traffic, including vehicles as heavy as a school bus, he said.

The board could take action on approving a contract this week, Clark said. After that, if the materials are available and weather permits, it could take about five days for the work, which includes two sets of vertical and horizontal beams and a steel deck, to be completed.

A spokesman for the railroad could not be reached Tuesday.

### What's Related

- Families welcome 3 babies on Christmas at Bassett
- Board bans heater usage
- Children meet person behind winter warmth
- Twelve Tribes to open cafe
- Bassett Hospital named a stroke center
- City discusses wood-plant plan
- A lifetime (or two) of service to 4-H
- Woman charged in \$96,000 embezzlement

The situation is something for the town and railroad to address, said state Department of Transportation spokeswoman Chris Klein. The state agency will inspect the bridge before it can be reopened.

Robert Decker lives on the other side of the closed bridge. He said Tuesday that until repairs are made, he has no choice but to travel what town officials said was about a five-mile detour to get to the hamlet.

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### CURRENT WEATHER

Conditions from The Daily Star: 102 Chestnut Street Oneonta, NY at 3:44p.m.:

**42.6°F**

Wind: WSW at 1MPH  
Today's High Temp: 45.5°F at 2:10p.m.  
Today's Low Temp: 36.3°F at 12:00a.m.

No current weather alerts

### POLL

Gov. Pataki claims we have a \$2 billion surplus. What do you think we should do with the funds?

- Pay down state debt
- Give the money to New York City for its schools
- Reduce our taxes
- Give raises to the state legislature

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### TODAY'S LOCAL NEWS

- Area schools ready for expanded state tests
- Boys who sat out tests in 2005 plan to take them now
- Teen artist wants to 'experiment' with upcoming

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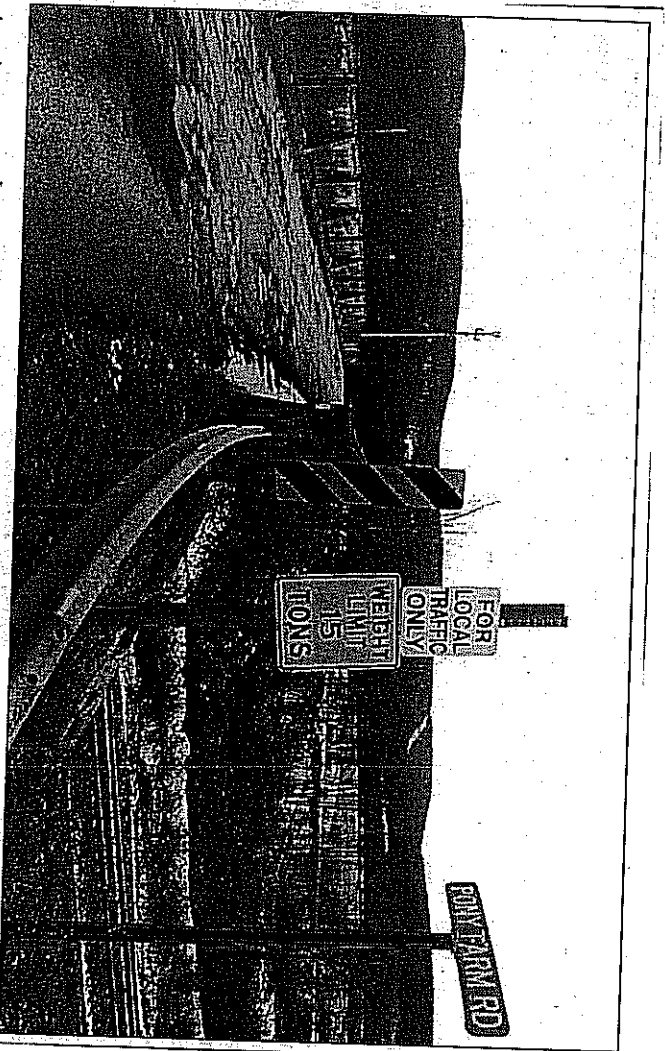
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Star photo by Julie Lewis

This bridge in the town of Oneonta, seen Wednesday, connects state Route 7 and Pony Farm Road while passing over Canadian Pacific Railroad tracks.

## Bridge in town of Oneonta closing

**WEST ONEONTA** — The bridge connecting Pony Farm Road and state Route 7 in the town of Oneonta is closing.

The bridge owned by Canadian Pacific Railroad was declared unsafe by the state Department of Transportation.

Barricades will be put in place Friday, said Oneonta Town Supervisor Robert Wood.

Traffic will be re-routed through Corpo-

rate Drive and the Pony Farm Industrial Park, Wood said.

It is not known how long the bridge will need to be closed or when it will be fixed, Wood said.

CP officials are conducting a study with state and local officials to determine the future of the bridge, according to a statement from CP spokesman Michel Spe-

nard.

**November 15, 2006**

**NYS Department of Transportation  
Lower Oneida Street  
Oneonta, NY 13820**

**Attn: Jerry Murrillo & Mike Adams**

**Subj; Unsafe Intersection @ Rt 7 & Pony Farm Rd**

**Dear Sirs,**

**Confirming my conversation with Jerry Murrillo, I am writing to express my concern with regards to safety at the subject intersection. Our employees & customers have expressed their concern & I've spoken with our residential neighbors & they agree something should be done.**

**When exiting from Pony Farm Rd onto Route 7 there is a problem that cars traveling east on Route 7 cannot be seen if they are just below the rise in the road. The same is true for cars traveling west on Route 7 & making a left hand turn onto Pony Farm Road.**

**There has been one accident & several near accidents at this intersection in the last few weeks. I look forward to meeting with you at 11 am on Friday, November 17 so we can look at this intersection together.**

**Please investigate options for better marking & lighting to reduce this hazard. I look forward to working with you to resolve this issue.**

**Bill Castine**

**August 7, 2006**

**Subj: Bridge Access to the West End of Pony Farm Road**

**Dear Board Members:**

**Thank you for the opportunity to express my concerns to the board on the subject matter..**

**My Concerns are both as an employer & on behalf of my residential neighbors. Our business is located at 330 Pony Farm Road. There are several businesses (approximately 25 employees total) located at this address; Commlnnovations, Village Printer, Solutions Integrators, AutoTech Electronics, UHaul & Century Storage. There are about a dozen residential homes along this portion of Pony Farm Road.**

**We are pleased to learn the repairs to flood damage bridge on Pony Farm Road across Otego Creek have been authorized & we look forward to seeing this road reopened this fall.**

**Our business is being affected in many ways:**

**Fire & other emergency vehicles exceeding 15 tons are prevented from using this access & the emergency only alternate at grade access is often blocked by railcars for extended periods.**

**Delivery & construction vehicles exceeding 15 tons cannot reach us at all, compromising our ability to conduct business & jeopardizing livelihood of our employees.**

**I have discussed this situation with my neighbors & they particularly concerned about the fact that the only route for fire & other emergency vehicles exceeding 15 tons is an at-grade crossing. This crossing is often blocked by stopped railroad cars. Even in an emergency it would take several minutes for a train to clear this path. Many of my neighbors are elderly & the homes are older wood frame structures. I'm sure you appreciate the importance of unimpeded access to emergency services.**

**Even before these floods we have seen the access across the Otego Creek bridge to this end of Pony Farm Road closed for periods of time due to much smaller floods than the recent occurrence. For this reason we would like to see the option for rebuilding the bridge & approach ramps much higher than the recent configuration.**

**Aside from floods access across the Otego Creek is blocked by road repair or heavy snow. For these reasons, the railroad bridge connecting Pony Farm Road**

**to Route 7 is of vital importance. This bridge was once rated as unlimited is now restricted to vehicles under 15 tons. Loss of this bridge would cause the loss of jobs adversely affect property values on this end of Pony Farm Road. It would also deprive the Industrial Park of its second entrance.**

**Improved lighting at this bridge is important to improve safety.**

**We were pleased to learn that you share our concern for this railroad bridge & that you will be working with Sen Seward & the CP Railroad. I look forward to attending future board meetings so that we can remain informed on progress.**

**Regards,      Bill Castine**

**August 7, 2006**

**Subj: Bridge Access to the West End of Pony Farm Road**

**Dear Sirs:**

**I am writing to you as a concerned employer & on behalf of my residential neighbors. Our business is located at 330 Pony Farm Road. There are several businesses (approximately 25 employees total) located at this address; Comminnovations, Village Printer, Solutions Integrators, AutoTech Electronics, UHaul & Century Storage. There are about a dozen residences along this portion of Pony Farm Road.**

**The recent floods have caused the bridge on Pony Farm Road across Otego Creek to be closed. We are concerned about seeing this road reopened as soon as possible.**

**Our business is being affected in many ways:**

**Fire & other emergency vehicles exceeding 15 tons are prevented from using this access & the emergency only alternate at grade access is often blocked by railcars for extended periods**

**Delivery & construction vehicles exceeding 15 tons cannot reach us at all**

**Even before these floods we have seen this access to this end of Pony Farm Road closed or impassible for periods of time due to floods, road repair or heavy snow. For this reason the railroad bridge connecting Pony Farm Road to Route 7 is of vital importance. This bridge was once rated as unlimited is now restricted to vehicles under 15 tons. Loss of this bridge would cause the loss of jobs adversely affect property values on this end of Pony Farm Road. It would also deprive the Industrial Park of its second entrance.**

**Improved lighting at this bridge is important to improve safety.**

**Other communities have found solutions to similar problems: the E. Worcester Bridge was repaired & the Otsego County Board of Representatives recently approved funding for the Susquehanna Avenue Bridge connecting Otsego & Middlefield.**

**We look forward to working together with you to find solutions for our bridge problem**

**Bill Castine**

